



CONCEPTUAL SCHEME

SW-33-52-26-W4M

Adopted by Parkland
County Council: **DATE**

Remington Acheson Zone 5 Conceptual
Scheme



ACKNOWLEDGEMENT PAGE

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1. INTRODUCTION

1.1 Purpose

The subject lands comprises ± 61.9 ha (± 153.12 ac) situated within the County's Acheson Industrial Area Structure Plan which is recognized as one of the fastest growing industrial areas in Western Canada featuring a range of businesses that leverage the County's (and Edmonton Region's) superior transportation logistics and distribution infrastructure (i.e., road, rail, and air).

As shown on **Figure 1: Regional Context**, the Plan area is located within an area of the County which is serviced by municipal potable water, sanitary, and stormwater infrastructure, and as such, is ideally suited to accommodate a comprehensively planned industrial business subdivision that accommodates transportation and logistics distribution facilities intended to support a variety of large format warehouse users.

Rogers Communications has owned and operated a communications tower within the subject lands for many years. Resulting from ongoing collaboration with Remington Development Corporation, Rogers has decided to isolate ownership of their tower and related facilities within an existing ± 10.8 ha (± 26.7 ac) parcel (Lot 1, Block 7, Plan 242 1654) and sell the balance of the quarter section to Remington, for the purposes of creating a new industrial business subdivision planned in accordance with the policy provisions of the Acheson Industrial Area Structure Plan.

1.2 Background

This Conceptual Scheme establishes a high-level vision and a proposed implementation policy framework to support the anticipated development of an industrial subdivision within the SW-33-52-26-W4M. This Plan contemplates a master-planned business development featuring fully-serviced industrial lands intended to accommodate large-format warehousing as well as potential opportunities for smaller-scale supportive services. The project is expected to attract business opportunities that benefit from the area's efficient access to the regional transportation network and availability of municipal utility servicing.

The Plan's policy framework considers the site's existing development opportunities and constraints, relevant municipal policy requirements including the Acheson Industrial Area Structure Plan, and recommendations of supporting technical reports prepared in support of this proposed development. The Plan's preparation process was supported by a stakeholder consultation process designed to provide interested parties the opportunity to receive information relative to the project and provide input accordingly.

This Conceptual Scheme is intended to be approved by Council resolution, and as such, will provide a non-statutory planning framework intended to inform future land use and subdivision applications within the Plan area.

1.3 Location and Plan Boundaries

As shown on **Figure 2: Local Context**, the Plan area is bound to the north by 96 Avenue, and to the west by 279 Street. The subject lands are situated ± 0.8 km ($\pm \frac{1}{2}$ mile) west of Highway 60, ± 0.8 km ($\pm \frac{1}{2}$ mile) south of Highway 16A, and ± 1.6 km (± 1 mile) north of Highway 628.

The boundary was chosen in consultation with Parkland County to provide logical planning to guide future development and align with the requirements of the County's Municipal Development Plan. The boundary was reduced from a full section of land to one quarter section due to all surrounding lands being fully developed for industrial purposes prior to the writing of this Conceptual Scheme.

Policy 1.3.1

Policies contained within the Conceptual Scheme shall apply to all lands within the Plan area.

1.4 Conceptual Scheme Objectives

The SW-33-52-26-W4M Conceptual Scheme is intended to:

- Summarize existing conditions and describe the site's development opportunities and constraints.
- Establish a future development concept with a land use framework designed to facilitate a master-planned industrial development in accordance with the Municipal Development Plan, Acheson Industrial Area Structure Plan, and other non-statutory County policies and procedures.

- Establish a strategy to implement appropriate transportation, utility service and stormwater management infrastructure necessary to support the anticipated development and related uses in accordance with the County's municipal engineering standards.
- Establish expectations for implementation of architectural controls to ensure coordinated treatment of future building design, landscaping, signage, and lighting treatments.
- Establish an anticipated development phasing strategy within the Plan area.
- Establish expectations for provision of fire, emergency response and community support services within the Plan area.
- Summarize the conclusions of a community consultation program implemented in support of the Plan's preparation process to inform & educate affected landowners and interested stakeholders.

1.5 Public Engagement

Engagement Plan

Remington Development Corporation is committed to consulting with the adjacent landowners and key stakeholders from the broader community to ensure that specific details relative to this proposed development are communicated openly and transparently in accordance with the following principles:

- To ensure all key stakeholders are identified and included in the process;
- To generate awareness about the Conceptual Scheme and provide opportunity for stakeholders to provide input;
- To present preliminary development plans for the development;
- To solicit and record concerns from key stakeholders so they can be proactively addressed during the Conceptual Scheme review process;
- To ensure stakeholders are kept informed of the Plan's progress and provided opportunity to review additional information if desired;
- To inform stakeholders how their input was used;
- To ensure the engagement process is monitored and measured, and results are shared with all stakeholders; and
- To conduct communications related to the proposed development in an open, honest and respectful manner

Identified Stakeholders

The following stakeholders were contacted throughout the engagement process as they were within an 800m radius from the site (apart from governing bodies/regulators and Canada Post).

Table 1: Identified Stakeholders

Parkland County	Parkland Courier Services
Supreme Steel	Freedom Cannabis
Rehoboth Christian Ministries	Landowner - 9156 279 St, Acheson, AB T7X 5A6
Bridgespan Café and Grill	AER
Pipeline Machinery International ULC	AEPA
STIHL	ATEC
Amazon YEG 2/YEG4	AHS
Bushell Transport Co.	ESRD (Water Act)

Parkland County	Parkland Courier Services
Clearwater Environmental Inc.	AltaLink
TCL Supply Chain Inc.	Canada Post
Acheson Industrial	Evergreen Catholic School Division
Equipment Corps – Acheson	Parkland School Division
Vermeer Canada	Fortis
Mustang Freightways	Telus
Rig Logistics Acheson	Telus, Outside Plant Engineering
Agate Tike	Atco Gas
JEN COL Construction	Acheson Business Association
Glowing Embers RV Park and Travel Centre	Alberta Capital Region Wastewater Commission (ACRWC)
IAA Edmonton	Capital Region Parkland Water Services Commission
Western Canadian Leadership Centre	

Virtual Engagement Program

The project team hosted a virtual public engagement program for the project from June 28 to July 12, 2024. The engagement program focused on sharing information about the project through a dedicated website, promoting the website and engagement program within the local area, and providing opportunities for the public to ask questions and provide feedback through the website and an online survey. For more detail, please refer to the "What We Heard Report".

Results of Virtual Engagement Program

Since public engagement began, only one (1) individual directly engaged using the online survey. No other forms of communication were experienced (i.e. no phone calls, emails, or letters were received). The one (1) participant advised that they were an employee/employer in the Acheson area and were engaged via the outdoor sign along the property. This individual shared that the landowner should prioritize Open Space, Wetland Protection & Maintenance, and Transportation and Access.

Summary of Virtual Engagement Efforts:

- 1 survey was completed
- 126 visits were made to the project website

2. POLICY FRAMEWORK

2.1 Edmonton Metropolitan Region Growth Plan

The Edmonton Metropolitan Region Growth Plan (“the Growth Plan”), effective 26 October 2017, provides the vision, guiding principles and six interrelated regional policy areas to guide the growth and development of the region over the next 30 years with a 50-year vision for a diverse, thriving, and competitive metropolitan region. The Growth Plan provides a comprehensive update and replaces the 2010 Capital Region Growth Plan: Growing Forward and integrates the policy directions from other Capital Region Board plans and studies. The Growth Plan emphasizes six regional policy areas with guiding principles and objectives focused on Economic Competitiveness and Employment, Natural Living Systems, Communities and Housing, Integration of Land Use and Infrastructure, Transportation Systems, and Agriculture.

The SW-33-52-26-W4M Conceptual Scheme aligns with achieving objectives of the guiding principles in the Growth Plan to define the policies and targets that respond to the Economic Competitiveness and Employment Policy Area, while addressing the unique growth challenges in the Region.

Policy 2.1.1 The Conceptual Scheme and the land development it generates shall conform to both the Edmonton Metropolitan Region Growth Plan and the Municipal Government Act.

2.2 Municipal Development Plan

Parkland County adopted the Municipal Development Plan (MDP) Bylaw 2017-14 in October 2017. The MDP includes the following vision statement:

Parkland County will steward our natural environment and foster a sustainable and diverse economy while maintaining our rural character. We will be innovative and creative in growing a healthy community that provides equitable opportunities for all, and we will celebrate our heritage and culture.

The MDP establishes a series of ‘community pillars’ which all future developments within the municipality are expected to consider including:

- Land Use
- Natural Environment
- Economy
- Community
- Governance

The MDP’s Economic Competitiveness and Employment Section seeks to facilitate Major Employment Areas to accommodate significant concentrations of business and economic activities for commercial, light industrial and medium industrial development with regional significance. This includes sustainable non-residential development within identified business areas and/or within hamlets.

Emphasis is placed on the support for new business development within Major Employment Areas which are intended to accommodate regionally and even nationally significant development within master-planned business parks that are supported by municipal infrastructure that is suited for the anticipated scale of operations. In doing so, the County continues to leverage its’ non-residential assessment base to ensure the municipality can achieve its fiscal sustainability objectives.

The SW-33-52-26-W4M Conceptual Scheme aligns with Policy 5.0.1 as it is supported with the County’s efforts to encourage economic and employment diversification where businesses will be supported by adequate infrastructure and transportation networks. This also includes the Conceptual Scheme’s alignment with the County encouraging a range of commercial and industrial lot sizes and servicing standards. Additionally, the Conceptual Scheme proposes to situate new business industrial development within an identified Major Employment Area (Policy 5.0.2) which is supported by an adopted statutory plan within vicinity of appropriate transportation & utility servicing infrastructure. Policy 5.0.2b specifically complements the SW-33-52-26-W4M Conceptual Scheme for it notes that the Major Employment Area will support a range of intensive land uses including commercial, industrial and institutional uses that require full municipal servicing. The Conceptual Scheme also aligns with Policy 5.0.4b as commercial and industrial uses

are encouraged to be located in developed areas where they are compatible with the surrounding land uses and can manage off-site impacts. This Conceptual Scheme's objectives are consistent with the MDP's growth management framework.

Policy 2.2.1 The Conceptual Scheme and the land development it generates shall conform to Parkland County's Municipal Development Plan.

2.3 Acheson Industrial Area Structure Plan

The County adopted the Acheson Industrial Area Structure Plan (ASP) Bylaw 2020-13 in 2020. The ASP's development strategy is based upon the six (6) following objectives:

- Establish an orderly and efficient approach to planning for the Acheson area that balances existing and future land uses.
- Capitalize on the strategic location of Acheson in the Edmonton Metropolitan Region to support the regional economy while being responsive to changing market trends.
- Establish policies that support future industrial and commercial "serviced" development in Acheson that are sensitive to non-industrial uses that currently exist within the Plan area.
- Recognize and protect the Wagner Natural Area Environmentally Significant Area and Recharge Zone.
- Enhance open space opportunities by maximizing the use of Municipal Reserve, Environmental Reserve and conservation areas for community and recreational uses and new trail networks.
- Recognize the importance of existing and future transportation and servicing plan, and the need to phase development in a systemic manner.

The Conceptual Scheme Plan area is situated within an identified industrial land use policy area with attendant policies that support the development of a Major Employment Area that looks to provide light industrial and medium industrial development within the Plan area to accommodate the growth of Acheson. The Acheson Industrial ASP looks to achieve this through the following "Relevant ASP Goals - Industrial":

- Support economical and orderly expansion of light and medium industrial development within the Plan area.
- Ensure industrial development is developed in such a manner as to be compatible with adjacent land uses within the Plan area.
- Protect provincial and municipal transportation corridors at time of development.

The Conceptual Scheme Plan aligns with Section 2.4 Industrial of the Acheson ASP, as reflected above in the "Relevant ASP Goals", as well as the ASP's Industrial Policies. The Conceptual Scheme meets both Policies 2.4.1 and 2.4.2, as no heavy industrial uses are proposed and the clustering of compatible businesses that support each other are proposed. The Conceptual Scheme is proposing a Light Industrial Land Use (Business Industrial District), which aligns with the neighbouring Light Industrial land uses as shown on Map 6: Future Land Use Concept (Policy 2.4.4). Though currently designated as a Constrained Land, the proposed development does not comply with this designation and is proposing to be amended in support of the development. The SW-33-52-26-W4M Conceptual Scheme development strategy is consistent with the development objectives and goals of the Acheson Industrial Area Structure Plan.

2.4 Land Use Bylaw

As shown on **Figure 4: Existing Land Use**, the Plan area is currently designated with Agricultural Restricted District (*AGR*) and Medium Industrial District (*M/I*) in accordance with the County's Land Use Bylaw 2017-18. Provided the existing land use districts surrounding the subject Plan area are largely designated as both Business Industrial and Medium Industrial Districts, the current designation of Agricultural Restricted District and Medium Industrial to the proposed Business Industrial District would seamlessly align with the surrounding districts. Through this change to a light industrial land use the Plan area will better align with overarching light industrial uses that surround the parcel, as reflected within both the MDP and ASP.

3. SITE CONTEXT

3.1 Legal Description and Ownership

As shown on **Figure 3: Legal Descriptions**, the study area includes an un-subdivided quarter section legally described as SW-33-52-26-W4M, as defined by **Table 2: Legal Descriptions**.

Table 2: Legal Descriptions

LEGAL DESCRIPTION	OWNER	AREA (HECTARES)	AREA (ACRES)
SW-33-52-26-W4M	RDC	51.05	126.15
Lot 1, Block 7, Plan 242 1654	Rogers Telecommunication	10.80	26.71
Road Plan 272 HW	Parkland County	1.16	2.87
TOTAL		63.01	155.73



3.2 Adjacent Land Uses

Adjacent parcels to the north and east contain lands designated Business Industrial District (*BI*) ranging from large-format warehousing to extensive lay-down storage facilities. Lands to the west, south, and southeast are designated as Medium Industrial District (*M*). One adjacent residential parcel exists southwest of the Plan area.

3.3 Site Conditions

As shown in **Figure 5: Existing Conditions**, the Plan area contains lands which are presently under cultivation.

3.3.1 Rogers Communications Tower

A telecommunications tower, owned and operated by Rogers Communication, is situated within the north central portion of the site (Lot 1, Block 7, Plan 242 1654). The tower enclosure contains a fenced area including an administrative office, warehouse, and related mechanical facilities. The tower is anchored by three guy-wires. This communications tower and supporting infrastructure will remain and will continue to be owned and operated by Rogers Communications.

3.3.2 Existing Access

As illustrated under Figure 5, primary access to the Plan area is provided via an existing approach off 96 Avenue – a paved municipal collector road maintained in good condition. A secondary access is provided in the southwest corner of the Plan via an approach off 279 Street (i.e., Range Road 264) giving access to a private driveway leading to the communications tower and related accessory buildings.

3.4 Historical Uses of the Site

In preparation for development, a Historical Resource Clearance Certificate was obtained. Alberta Historic Resources issued clearance and approval for SW-33-52-26-W4M relative to the Historic Resources Act:

Table 3: Historical Resource Clearance

APPROVAL	APPROVAL NO.	ISSUE DATE	LANDS COVERED	APPROVAL HOLDER
Historical Resources Act	4835-24-0087-001	September 11, 2024	SW 33-52-26-W4M	Remington Development Corporation

Policy 3.4.1

Should any historical resources be encountered during construction, Parkland County and Alberta Culture and Tourism (Historic Resource Management Branch) shall be contacted immediately.

4. PHYSICAL SITE FEATURES

4.1 Topography

Topography within the Plan Area is undulating with very slight grades sloping generally from south towards north. As illustrated on **Figure 6: Topography**, the subject land includes a number of ephemeral drainages and low-lying areas that collect seasonal drainage. Elevation within the extreme southwest corner of the site falls to a low point of 705m. This area corresponds to an existing wetland and treed area situated within the adjacent parcel.

4.2 Geotechnical Considerations

A **Preliminary Geotechnical Investigation** was prepared in support of this Conceptual Scheme. Geotechnical conditions in the Plan area are considered favourable for this proposed development. It is acknowledged that a geotechnical investigation will be completed by the developer within the Plan area at the subdivision stage, and any site-specific issues identified by this study are expected to be addressed through the detailed engineering design process and implementation of construction best management practices.

Policy 4.2.1

The Developer shall submit a detailed geotechnical analysis at the detailed design stage, to be prepared by a qualified Geotechnical Engineer, to confirm the suitability of subsurface conditions in accordance with the requirements of Parkland County's Engineering Design Standards.

4.3 Environmental Considerations

Comprehensive Biophysical Assessment

A Biophysical Assessment was prepared in support of the Conceptual Scheme by WSP in February 2024. The Plan area contains five (5) identified wetlands with none being claimed by the Crown (i.e., Public Lands). Except for the wetland within the southwest of the site (W05), and the wetland within the Rogers Tower Site (W03), all identified wetlands are expected to be removed to accommodate the proposed development. It is acknowledged that wetland disturbances must proceed in accordance with the procedural, technical, and compensation requirements established by the Alberta Provincial Wetland Policy to the satisfaction of Alberta Environment & Protected Areas. Further, retaining portions of Habitat Patch (HP01) should be considered at the subdivision stage to maintain connectivity between the Plan Area and existing adjacent habitat patches situated to the west and south.

Policy 4.3.1

Subdivision applications and development permits within the Plan area shall be consistent with the recommendations outlined in the Biophysical Assessment (WSP – February 2024), to the satisfaction of Parkland County.

Wetland Assessment and Impact Report

A Wetland Assessment and Impact Report was prepared within the Plan area by WSP Canada Inc. This report noted that Five wetlands (W01, to W05) and two ephemeral waterbodies (E01 and E02) were confirmed within the Plan area from preliminary wetland mapping during the desktop review and field survey. Portions of these features will be permanently disturbed during construction of the Project.

A total of 3.14 ha of wetland area in three wetlands (W01, W02, W04) will be directly and permanently impacted by the Project. The proposed wetland losses will result despite careful analysis and design that have considered the feasibility for avoidance. Development within W05 will be avoided by the intentional design for the extension of 279 Street to avoid this wetland. The Rogers Lands parcel (Lot 1, Block 7, Plan 242 1654) will not be developed by the Proponent and wetland W03 will also be avoided. Post-development water management will be supported by the construction of a stormwater pond. The Proponent proposes to compensate for permanent wetland losses through an in-lieu replacement fee payment of \$103,102.76.

The Plan area and surrounding lands have been influenced by current and historical land use and anthropogenic disturbances associated with agricultural and industrial use. Mitigation measures (e.g., silt fence) and BMPs will be used to reduce potential direct and indirect effects off-site. No further permanent wetland losses are anticipated inside the Plan area. Furthermore, to reduce the risk of indirect effects any wetlands not removed during Project construction, the Proponent is committed to the use of mitigations as outlined in Section 4.2 of the report.

Policy 4.3.2 Subdivision applications and development permits within the Plan area shall be consistent with the recommendations outlined in the Wetland Assessment and Impact Report (WSP – April 2024), to the satisfaction of Parkland County.

Environmental Site Assessment

A Phase 1 Environmental Site Assessment (ESA) was prepared within the Plan area by Cirrus Environmental Services Inc. This report concluded that the subject lands contain no evidence of contamination that would constrain the redevelopment of the site with industrial land use and subdivision.

4.4 Existing Transportation and Utility Considerations

A road diversion plan was surveyed and registered in 1945 to accommodate the potential construction of the portion of Range Road 264 within an alignment that avoids negatively impacting the wetland on the adjacent parcel. Neither this road diversion plan, nor the 20m statutory road allowance directly west of the Plan area have been developed.

The Plan area is presently serviced with electricity and telephone via a utility service extending from 96 Avenue to the Rogers Tower communication site. This infrastructure will be relocated at the subdivision stage.

Altalink and Fortis own and operate linear infrastructure immediately west of the 279 Street statutory road allowance (within the adjacent parcel to the west of the Plan area).

4.5 Oil and Gas Infrastructure

The Plan area does not contain any infrastructure related to the oil & gas sector.

An abandoned well site is located within the adjacent parcel immediately east of the Plan area.

5. DEVELOPMENT CONCEPT

5.1 Land Use Concept

The **SW-33-52-26-W4M Conceptual Scheme** contemplates a master-planned industrial business park intended to accommodate large format industrial warehouse uses that leverage proximity to the County's regional transportation network and municipal utility servicing and stormwater drainage network. Future development within the Plan area is expected to accommodate business development that are consistent with the objectives of the County's Acheson Industrial Area Structure Plan and Municipal Development Plan.

As illustrated on **Figure 7: Development Concept**, the Plan area is proposed to include eight (8) new industrial parcels, internal road rights-of-way, a public utility lot (PUL) for stormwater management and related infrastructure, and an Environmental Reserve (ER) to accommodate an existing low-lying portion of the site that is connected to an existing wetland facility within the adjacent lands. The existing Roger's communications tower located on Lot 1, Block 7, Plan 242 1654 will continue to be owned and operated by Rogers Communications.

The Plan area will be accessed via extension of 279 Street along the western boundary of the site in addition to an internal industrial subdivision road connecting 96 Avenue with 279 Street. All transportation infrastructure will be designed and constructed by the developer in accordance with the County's Engineering Design Standards. Potable water service will be provided via extension of the Capital Region Water System and wastewater service will be provided via a gravity system connecting into the Parkland Sanitary Trunk. The developer will design and construct all required utility infrastructure in accordance with the County's Engineering Design Standards.

Stormwater management will be provided via an overland drainage system that will convey surface flows to a stormwater facility to be constructed within a public utility lot (PUL) situated in the central portion of the site. This stormwater facility will be designed to retain drainage generated from each business lot and the road rights-of-way. A Lift Station will be constructed to convey surface drainage from the PUL to the Acheson Stormwater Trunk situated within the 96 Avenue road right-of-way.

Architectural controls will be established by the developer at the subdivision stage to ensure the facades of buildings and design of outdoor storage areas are consistent with the Building and Site Design Guidelines of the Acheson Industrial Area Structure Plan.

A Business Lot Owner Association (BLOA) will be established at the subdivision stage to manage various services within the project such as waste removal, recycling, and implementation of architectural controls.

The SW-33-52-26-W4M Conceptual Scheme Plan area is anticipated to be developed in a single phase, likely taking 2 – 5 years to fully build out, subject to the availability of infrastructure and market demand.

The anticipated land use and associated development statistics to be established within the Plan area is described in **Table 4: Development Statistics**.

5.2 Development Statistics

Table 4: Development Statistics

LAND USE STATISTICS	AREA (HA±)	AREA (AC±)
Business Industrial (BI)	46.98	116.08
Medium Industrial (MI)	10.80	26.68
Road Right-of-Way	5.27	13.01
Total Plan Area	63.04*	155.77*

* Estimates using AutoCAD measurements. Discrepancies with Table 2: Legal Descriptions will be confirmed at the subdivision stage.

5.3 Municipal Reserve and Open Spaces

The open space system within the Plan area will include a public utility lot (PUL) and Environmental Reserve (ER) as generally illustrated on **Figure 12: Open Space**.

5.3.1 Public Utility Lot (PUL)

A Public Utility Lot (PUL) will be dedicated to the County to accommodate stormwater management, as generally illustrated on **Figure 12: Open Space**.

5.3.2 Municipal Reserve (MR)

Dedication of municipal reserve (MR) within the Plan area shall be provided by the developer at the subdivision stage via payment of cash-in-lieu of land in accordance with the requirements of the Municipal Government Act. The amount of municipal reserve outstanding within the Plan area and anticipated dedication is summarized in **Table 5: Anticipated MR Dedication**.

Table 5: Anticipated Municipal Reserve (MR) Dedication

DESCRIPTION	AREA (HA±)	AREA (AC±)
Gross Development Area	61.97	153.12
Less area to be dedicated Environmental Reserve (ER)	0.86	2.12
= Net Developable Area (NDA)	61.11	151
Amount of MR Outstanding (=10% of NDA)	6.1	15.1

Policy 5.3.2.1 In accordance with Parkland County Policy C-PD15, dedication of Municipal Reserve (MR) shall be provided by the developer prior to subdivision endorsement via cash-in-lieu payment pursuant to the provisions of the Municipal Government Act.

5.4 Environmental Reserve and Wetland Compensation

5.4.1 Environmental Reserve (ER)

An Environmental Reserve (ER) of ±0.96 ha (2.37 ac) will be dedicated to a portion of lands on the west side of the site area, west of the 279 Street Road diversion.

5.4.2 Wetland Compensation

Table 6: Wetland Compensation

WETLAND ID	WETLAND CLASSIFICATION	TOTAL WETLAND AREA (HA)	WETLAND AREA INTERSECTED BY PROJECT (HA)	% OF WETLAND LOSS	TOTAL CATCHMENT AREA (HA)	CATCHMENT AREA INTERSECTED BY PROJECT (HA)	% OF CATCHMENT IMPACTS
W01	Seasonal graminoid marsh	2.01	2.01	100	42.0	20.51	49
W02	Temporary graminoid marsh	0.12	0.12	100	3.15	2.48	79
W03	Temporary graminoid marsh	0.16	0	0	4.04	0.55	14
W04	Seasonal graminoid marsh / seasonal shrubby swamp	1.02	1.02	100	10.12	7.63	75

WETLAND ID	WETLAND CLASSIFICATION	TOTAL WETLAND AREA (HA)	WETLAND AREA INTERSECTED BY PROJECT (HA)	% OF WETLAND LOSS	TOTAL CATCHMENT AREA (HA)	CATCHMENT AREA INTERSECTED BY PROJECT (HA)	% OF CATCHMENT IMPACTS
W05	Seasonal graminoid marsh	0.25	0	0	2.21	0.18	8
TOTAL		3.56	3.14	-	61.5	31.35	-

- Policy 5.4.1*** Wetland disturbances shall proceed in accordance with the procedural, technical, and compensation requirements established by the Alberta Provincial Wetland Policy to the satisfaction of Alberta Environment & Protected Areas prior to subdivision endorsement.
- Policy 5.4.2*** The developer shall obtain the necessary Water Act approvals prior to construction and/or development permit where applicable to the subject lands and such approvals shall be provided to Parkland County.
- Policy 5.4.3*** Wetland compensation arrangements with the Province shall be established prior to subdivision application where applicable to the subject lands.
- Policy 5.4.4*** Any clearing of vegetation should occur between September 1 through to January 31 and clearing of vegetation beyond this timeframe shall require the employment of a qualified biologist to verify that no nesting migratory birds are harmed or disturbed.

6. TRANSPORTATION

Transportation Concept in accordance with the Acheson and Big Lake Traffic Impact Assessment, September 2016.

6.1 Transportation Concept

As part of the TIA background information review, McElhanney reviewed the Acheson/Big Lake TIA (Bunt & Associates, 2018) as the key base for technical traffic information. Road Classifications, Traffic Volumes, Intersection Configurations and other technical information defined within the Near-Term, Long-Term and Ultimate Horizon's were used as the basis for our background traffic volumes and transportation parameters. Since the Acheson/Big Lake TIA did not consider development of this site until the Ultimate Horizon, site generated volumes from this development were added to the background information in the near-term to provide the opening day projections (Acheson TIA – 2027 Horizon / Rogers TIA - Opening Day Horizon). To ensure conservative estimates, exact trip generation for this site was also added to the Ultimate Horizon (2047 Horizon).

Several variations from this network TIA were communicated to the consulting team from Parkland County including the introduction of signalized intersections on Highway 16A, new roadway classification preferences, and access management standards which were considered as part of the SW-33 Industrial TIA. This information was used to re-balance the volumes and provide updated recommendations for accesses and intersection configurations based on County input.

The implementation of this development confirmed the requirement for 279 Street to be completed and in addition, proposed a new internal roadway to minimize accesses directly onto the adjacent roadways. No other transportation network changes were proposed as part of this development (excluding site accesses).

Other documents that were considered included the Parkland County TMP, Highlands Business Park TIA and 92 Avenue & 279 Street Concept Plans.

6.2 Traffic Impact Assessment (TIA)

A Traffic Impact Assessment (TIA) was prepared by McElhanney in support of this Conceptual Scheme to evaluate the impacts of the proposed development on the surrounding municipal and regional transportation network and to recommend the necessary road improvements (if any) required to maintain acceptable service levels along the adjacent road network over the next 20 years. The TIA evaluated the proposed development's potential affect on the following five (5) existing intersections:

- Highway 60 & 96 Avenue
- 96 Avenue & 266 Street
- 96 Avenue & 279 Street
- Highway 16A & 279 Street
- 92 Avenue & 279 Street

The TIA assumes construction of the industrial development will occur in 2025 with operations commencing by 2026. The TIA also assumes a 'future' scenario at the 2047-time horizon.

The conclusions of the TIA indicate that, while the additional traffic generated by industrial development within the Plan area will have some impact to the level of service (LOS) affecting the associated intersections, these anticipated impacts are considered to be acceptable at opening day and within the future 2047-time horizon.

The developer acknowledges that construction of 279 Street will be required to support development within the Plan area. The portion of the municipal grid network will be completed by the developer to connect 96 Avenue and 92 Avenue as illustrated on **Figure 8: Transportation**. This road improvement is anticipated to include a 2-lane industrial roadway, to be designed and constructed to the satisfaction of the County.

Policy 6.2.1 Access will be provided to the Plan area as generally illustrated on Figure 8: Transportation.

Policy 6.2.2 The developer shall construct the portion of 279 Street between 92 Avenue and 96 Avenue as a two-lane industrial road, at the subdivision stage, to the satisfaction of Parkland County.

<i>Policy 6.2.3</i>	The future roadway network and key intersections in the Plan area shall be designed and constructed to accommodate all roadway users at a safe and satisfactory level of transportation services.
<i>Policy 6.2.4</i>	Subdivision applications and development permits within the Plan area shall be consistent with the recommendation as outlined in the Traffic Impact Assessment (McElhanney – October 31, 2024, as amended), to the satisfaction of Parkland County.
<i>Policy 6.2.5</i>	The Developer shall provide payment of Parkland County's Off-Site Transportation Levy at the subdivision and/or development permit stage.

6.3 Internal Vehicular Circulation and Access

Access within the Plan area will be provided by the developer's construction of an internal subdivision road network as generally illustrated on **Figure 8: Transportation**.

Access will be provided by a new intersection to be constructed off of 96 Avenue. As described within the TIA, to address westbound safety concerns, it is recommended that this intersection include a left-turn bay with a minimum 50 m storage area. A second intersection will be constructed off 279 Street as illustrated on **Figure 8**. No signalization of these intersections will be required.

It is anticipated that all internal subdivision roads within the Plan area are expected to include paved road surfaces to be designed and constructed in accordance with the County's Engineering Design Standards.

Given the industrial nature of the proposed development within the Plan area, the accommodation of active modes infrastructure (i.e. sidewalks) is not expected within the internal subdivision road network. All on-site parking areas are expected to be contained within each lot's subdivision boundary without impacts to the external road network.

<i>Policy 6.3.1</i>	The design of the internal subdivision road network, including intersections with the surrounding municipal road network, shall be established at the subdivision stage in accordance with the County's Engineering Design Standards. Changes to the internal road layout identified in Figure 8: Transportation shall not require amendments to this Conceptual Scheme, as long as the internal road is classified as a Local Road. Any changes to a Collector Road shall require an amendment to this Conceptual Scheme.
<i>Policy 6.3.2</i>	All dead-end roads, whether temporary or permanent, shall include a cul-de-sac turnaround, with a minimum radius as required under Parkland County's Engineering Design Standards.

7. SERVICING SCENARIO

Utility Servicing Concept in accordance with the Acheson and Big Lake Water Servicing Study, 2015 and the Acheson and Big Lake Sanitary Servicing Study, 2016.

7.1 Utility Servicing Design Brief

A Utility Servicing Design Brief was prepared by V3 Companies in support of this Conceptual Scheme to evaluate the requirements of extending municipal servicing into the Plan area.

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| Policy 7.1.1 | The developer, prior to subdivision endorsement, shall be required to enter into a Development Agreement with Parkland County regarding all off-site infrastructure and construction. |
| Policy 7.1.2 | Subdivision applications and development permits within the Plan area shall be consistent with the recommendations outlined in the Sanitary and Water Servicing Report (V3 Companies – October 18, 2024), to the satisfaction of Parkland County. |

7.2 Potable Water Service

As generally illustrated on **Figure 9: Water Servicing**, the Plan area will be serviced with potable water via connection with the Parkland County water system.

Currently, there is an existing 400mm water main along 96 Avenue and a 300 mm water stub has been provided south into the proposed development. The water stub is near the location of proposed Road B. There is also an existing 400mm water main within the undeveloped 279 Street road right-of-way. Parkland County Engineering Design Standards have been used for the water demand calculations of the proposed development.

Development within the Plan area will be provided water service by a looped extension of the existing water system, to be constructed by the developer at the subdivision stage, via a 300mm water main along both Road A and Road B from 279 Street in the southwest to 96 Avenue in the northeast, as generally illustrated on **Figure 9: Water Servicing**.

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| Policy 7.2.1 | Potable water service shall be provided within the Plan area as generally illustrated by Figure 9: Water Servicing. |
| Policy 7.2.2 | The water distribution network shall be designed and constructed by the developer at the subdivision stage in accordance with the Parkland County Engineering Design Standards and the most current Acheson and Big Lake Area Water Servicing Study. |
| Policy 7.2.3 | The water distribution network shall be owned and maintained by the County. |
| Policy 7.2.4 | The design of the water distribution network shall accommodate fire suppression in accordance with the Parkland County Engineering Design Standards. |
| Policy 7.2.5 | At the time of subdivision, the Developer shall provide modelling data for water services. This data will be provided to the County's engineering consultant maintaining the servicing models for the Acheson area, and any required improvements identified in the updated model shall be required as a condition of subdivision approval. |
| Policy 7.2.6 | The Developer shall provide payment of Parkland County's Off-Site Water Levy at the subdivision and/or development permit stage. |

7.3 Wastewater Service

As generally illustrated on **Figure 10: Wastewater Servicing**, the Plan area will be serviced with wastewater via connection with the County's sanitary sewer line.

Currently, there is an existing sanitary sewer infrastructure located at the northwest corner and northeast portion of the proposed development. The existing infrastructure consists of an existing 250 mm sanitary sewer at the intersection of

96 Avenue and 279 Street, and an existing 450 mm sanitary sewer along 96 Avenue. The guidelines outlined in the Parkland County Engineering Design Standards have been used for the sanitary demand calculations of the proposed development.

Development within the Plan area will be provided wastewater service via extension of the existing sanitary sewer south from the intersection of 279 Street and 96 Avenue along the future extension of 279 Street and then east into the development area. This extension will service the northwest, central, and south areas of the Plan. The second extension will be across 96 Avenue into the development to service the northeast area of the Plan and via the existing 450 mm sanitary sewer as generally illustrated by **Figure 10: Wastewater Servicing**. The wastewater service will be provided with a gravity system, negating the need for a sanitary lift station within the subject lands as is identified within the Acheson/Big Lake Sanitary Servicing Study.

Policy 7.3.1	Wastewater service shall be provided within the Plan area as generally illustrated by Figure 10: Wastewater Servicing.
Policy 7.3.2	The wastewater collection network shall be designed and constructed by the developer at the subdivision stage in accordance with the Parkland County Engineering Design Standards and the most current Acheson and Big Lake Area Sanitary Sewer Servicing Study.
Policy 7.3.3	The wastewater collection network shall be owned and maintained by the County.
Policy 7.3.4	At the time of subdivision, the Developer shall provide modelling data for sanitary services. This data will be provided to the County's engineering consultant maintaining the servicing models for the Acheson area, and any required improvements identified in the updated model shall be required as a condition of subdivision approval.
Policy 7.3.5	The Developer shall provide payment of Parkland County's Off-Site Sanitary Sewer Levy at the subdivision and/or development permit stage.

7.4 Stormwater Management

Stormwater Management Concept in accordance with the Acheson and Big Lake Master Drainage Plan, 2011. Fenceline Boundary Water Act Approval No. 00287756-00-00

At present, the rainfall runoff on the existing property is split between a depression on the northwest-side of the site and a depression on the east side of the site. The surrounding lands of the proposed Tower Industrial Park appear to drain away from the development. 279 Street currently drains to localized wetland pothole depressions.

The proposed development will include a stormwater management facility to control the runoff from the site. The pond will be constructed in the central region of the development and will handle 60.68 hectares of the site. The remaining 1.23 hectares of the site will flow west into the existing wetland. The pond will include a lift station to pump runoff at a controlled rate into the storm force main in 96 Avenue. The guidelines outlined in the Parkland County Engineering Design Standards and the Alberta Environment Stormwater Management Guidelines have been used for the design of the proposed storm water management infrastructure and facilities.

Policy 7.4.1	Stormwater management shall be provided within the Plan area as generally illustrated on Figure 11: Stormwater Management. The final location and size of the stormwater pond shall be determined at detailed design.
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7.4.1 Stormwater Management Report

A Stormwater Management Report was prepared in support of the Conceptual Scheme. The report recommends a stormwater management facility will be constructed by the developer as generally illustrated on **Figure 11: Stormwater Management**.

The stormwater management facility will include a detention wet pond designed in accordance with the requirements of the County's Engineering Design Standards. Captured runoff will be stored and treated within a Public Utility Lot (PUL) to be dedicated to the County at the subdivision stage. Runoff from the PUL will discharge through a lift station and

internal force main connection to the County's existing force main situated within 96 Avenue.

The stormwater management facility will be designed to contain a 100-year 24-hour rainfall event with an allowable release rate of 0.6 L/s/ha for the associated tributary area. The pond will be designed to release a maximum of 36.4 L/s (0.6 L/s/ha x 60.68 ha).

The pond facility is proposed have 5 to 1 side slopes proposed from the free board level until 1 m below the normal water level (NWL), and maximum 3 to 1 side slopes below that to the pond bottom. As these side slopes do not conform to the County's Engineering Design Standards, a variance will be requested at the subdivision stage.

Where required, ditches conveying runoff from the lots towards the PUL will be placed within easements so that proper maintenance and upkeep can be done by the municipality rather than left to individual lot owners.

All public infrastructure associated with the stormwater management system shall be owned and maintained by the County. The developer shall register an overland drainage right-of-way plan in favour of the County to assign rights for the municipality to gain access to this infrastructure in the event of emergency or to ensure required maintenance activities are completed.

The developer will provide payment of off-site levies at the subdivision stage in accordance with the County's current Stormwater Off-site Levy.

Policy 7.4.2	The developer shall provide a Site-Specific Stormwater Implementation Plan (SSIP) at the detailed design stage to ensure positive drainage conditions are maintained within the Plan area.
Policy 7.4.3	The design of the stormwater management system within the Plan area shall be in accordance with AEP Approval 00287756-00-00 'Fenceline Boundary Agreement' and shall accommodate the unit area release rates and volume retention targets established by the Acheson and Big Lake Master Drainage Plan, 2011.
Policy 7.4.4	The stormwater management facility shall be designed and constructed by the developer at the subdivision stage in accordance with the Parkland County Engineering Design Standards.
Policy 7.4.5	The stormwater management system shall be designed and constructed to meet or exceed Parkland County's Engineering Design Standards as well as Alberta Environment and Protected Area stormwater management and stormwater quality requirements for the Acheson Big Lake Storm Basin.
Policy 7.4.6	The stormwater management facility shall be dedicated to Parkland County within a Public Utility Lot (PUL) at the subdivision stage.
Policy 7.4.7	All stormwater management infrastructure within the Plan area will be owned and operated by the County.
Policy 7.4.8	At the time of subdivision, the Developer shall provide modelling data for stormwater services. This data will be provided to the County's engineering consultant maintaining the servicing models for the Acheson area, and any required improvements identified in the updated model shall be required as a condition of subdivision approval.
Policy 7.4.9	The Developer shall provide payment of Parkland County's Off-Site Stormwater Levy at the subdivision and/or development permit stage.

7.5 Community Support Services

7.5.1 Fire Response

Fire response within the Plan area is expected to be provided from the Acheson Fire Station. The specific mechanism to provide fire response within the Plan area will be established at the detailed design stage after conditional subdivision approval.

7.5.2 Police Response

Police response will be provided by the RCMP Detachment in the City of Spruce Grove with support from the County Peace Officers.

7.5.3 Emergency Response

Emergency response will be accommodated by the 911 system with dispatch of ambulance service from EMS facilities located within the Town of Stony Plain.

7.5.4 Waste Management

The developer and/or a Business Lot Owner Association will contract solid waste management as required by development within the Plan area to a qualified waste management service provider.

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| <i>Policy 7.5.4.1</i> | The developer shall prepare a Waste Management Plan at the detailed design stage, to the satisfaction of the County. |
| <i>Policy 7.5.4.2</i> | The developer shall establish A Business Lot Owner Association at the detailed design stage to manage a contract with a qualified waste management provider. |

7.6 Shallow Utilities

Shallow franchise utilities will be provided within the Plan area by the developer at the subdivision stage in accordance with the requirements of the applicable shallow utility providers and Parkland County's Engineering Design Standards.

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| <i>Policy 7.6.1</i> | Shallow franchise utilities shall be installed and/or financed by the developer at the subdivision stage in consultation with the applicable utility providers. |
| <i>Policy 7.6.2</i> | The alignments for franchise utility installations shall be determined at the subdivision stage in accordance with Parkland County Engineering Design Standards and the requirements of the applicable franchise utility operator. |

7.7 Site Grading

The Plan area will be graded in the following manner:

- Lots on the east side will drain from east to west towards the new road and then south along ditches towards the proposed storm water management facility.
- Lots on the south will drain from south to north towards the new road and the east along ditches towards the proposed storm water management facility.
- Central lots will drain from south to north towards a ditch in a PUL that is draining east towards the proposed storm water management facility.
- Lots on the west side will drain from east to west towards 279th Street and then south along ditches towards the proposed storm water management facility.
- The remaining Rogers property (Lot 1, Block 7, Plan 242 1654) will be maintained except for the common property boundaries where grading adjustments are necessary.

It is intended to grade the development to obtain an overall earthworks balance while meeting perimeter constraints with existing developed properties. Existing drainage patterns are being maintained as part of the overall development.

A grading plan with an erosion and sediment control report will be prepared in accordance with Parkland County requirements depicting necessary road and swale grades. It will also identify Public Utility Lots (PUL), Storm Water Management Facilities (SWMF) and subdivided lots. A stripping and grading permit will be obtained prior to any earthworks being undertaken.

- Policy 7.7.1*** Prior to stripping and grading within the Plan area, a grading plan and erosion and sediment control report shall be submitted to the satisfaction of Parkland County. This plan shall depict road and swale grades and include public utility lot areas, stormwater management plans, and an erosion and sediment control plan and shall be approved by Parkland County.
- Policy 7.7.2*** A stripping a grading permit issued by Parkland County shall be in place prior to any soils being removed from the Plan area.

8. IMPLEMENTATION

8.1 Proposed Amendment to the Acheson Industrial Area Structure Plan

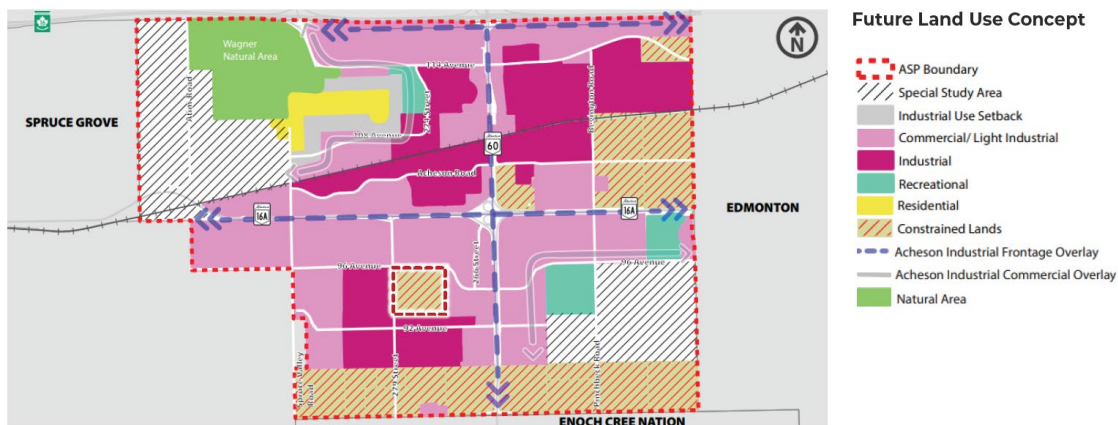
The Acheson Industrial Area Structure Plan establishes a comprehensive, high level planning framework to guide the continued and future business development in an orderly and efficient manner within the Plan area. Adopted by Council in 2020, The ASP encourages a variety of employment opportunities and is responsive to changing local and regional economic trends, while recognizing the ecological benefits of environmentally significant areas.

The **Acheson Industrial Area Structure Plan's Map 6: Future Land Use Concept** classifies the subject lands as parcel with 'constrained lands' - which means lands where future development is delayed due to current land uses or servicing constraints. Lands in this category will be reviewed for their development potential via updates of the ASP.

Rogers Communications has owned and operated a communications tower within the quarter section for many years. To avoid potential conflict arising from adjacent developments, Rogers previously determined that they did not want to allow potential encroachment of industrial development within proximity to their infrastructure. However, Rogers and Remington Development have evaluated the potential risks associated with development to proceed within the balance of the SW-33-52-26-W4M. Subsequently, Rogers has decided to isolate ownership of the lands immediately surrounding the communications tower (Lot 1, Block 7, Plan 242 1654) and continue operating the communications tower for the foreseeable future. Subsequently, they have decided to sell the balance of the quarter section to Remington to develop an industrial business park featuring large format transportation and logistics warehousing. In order to implement the development concept proposed by this Plan, an amendment to the Acheson Industrial Area Structure Plan is required to reclassify the subject lands from a 'constrained lands' category to 'commercial / light industrial' category. An application to amend the ASP will be submitted by the developer concurrent with the Conceptual Scheme application.

Policy 8.1.1

Prior to any land development activities occurring within the Plan area, amendments to the designation of the lands within Map 6: Future Land Use Concept of the Acheson Industrial Area Structure Plan shall be completed to ensure the future land use designation is consistent with the designated purposes outlined in Figure 7: Development Concept of this Conceptual Scheme.



8.2 Proposed Amendment to the Land Use Bylaw (Districting)

Land use within the Plan area will be assigned by Council in accordance with the Parkland County Land Use Bylaw. The developer's intent is to have Council consider the required land use changes within the site concurrently with the project's overall Conceptual Scheme phasing strategy. The land use amendment is expected to be evaluated concurrently with this Conceptual Scheme as generally illustrated on **Figure 13: Proposed Land Use** with key considerations described generally as follows:

- Industrial areas of the project will be designated Business Industrial District (BI).
- The Rogers Communications Tower site shall remain designated Medium Industrial District (MI).

Policy 8.2.1

Prior to any land development activities occurring within the Plan area, amendments to the districting of Parkland County's Land Use Bylaw shall be completed to ensure the districting is

consistent with the designated purposes outlined in Figure 13: Proposed Land Use of this Conceptual Scheme.

8.3 Proposed Subdivision

Implementation of the initial phase of subdivision within the Plan area is expected to occur as generally illustrated by **Figure 14: Proposed Subdivision** according to specific development criteria described as follows:

- Approximately eight (8) industrial lots ranging in size from ± 3.24 ha (± 8 ac) to ± 16.6 ha (± 41 ac) in addition to the existing ± 10.8 ha (26.68 acres) Lot 1, Block 7, Plan 242 1654 containing the existing Rogers communication tower;
- Dedication of public road rights-of-way, Public Utility Lot (PUL), Environmental Reserve (ER), and payment of cash-in-lieu of land for Municipal Reserves (MR); and
- Implementation of all required transportation & utility servicing infrastructure.

Policy 8.3.1 Subdivision within the Plan area is expected to proceed as generally illustrated on Figure 14: Proposed Subdivision.

Policy 8.3.2 Future lot sizes and configurations shall be determined at the time of subdivision. Changes to the proposed lot layout identified in Figure 14: Proposed Subdivision shall not require amendments to this Conceptual Scheme.

8.4 Development Phasing

Development within the Plan area is expected to proceed in one (1) phase as generally illustrated by **Figure 15: Phasing**. Notwithstanding, the developer may implement subdivision and development within the Plan area according to an alternate phasing strategy without the need to amend this Conceptual Scheme provided that appropriate infrastructure can be provided to support the development phase, including any temporary second access/egress point requirements if needed.

Policy 8.4.1 The development within the Plan area is expected to proceed in one (1) phase as generally illustrated on Figure 15: Phasing.

Policy 8.4.2 An alternate phasing program may be permitted without an amendment to this Plan provided there is appropriate infrastructure available to support each phase, including any temporary second access/egress point requirements if needed.

8.5 Business Lot Owners Association

Policy 8.5.1 A Business Lot Owner's Association shall be established at the detailed design stage for the purposes of managing a contract with a waste management provider and other duties as required.

8.6 Architectural Design Considerations

The SW-33-52-26-W4M Conceptual Scheme contemplates the creation of a master-planned industrial park that will be attractively designed, integrate with existing adjacent developments, respect the County's Commercial, Office and Industrial Design Guidelines.

The specific type and scale of development anticipated within Plan area is expected to include business operations of varying types, scale, and intensity. However, in all cases, business development within the Plan area is expected to establish and maintain an attractive development form.

The developer shall prepare architectural guidelines at the detailed design stage application approval stage to ensure the form of development maintains a coordinated and cohesive built form. The architectural guidelines will establish design criteria including the following:

- Building form & character (i.e., architectural theming);
- Appropriate articulation of building massing, facade treatment, and exterior materials & colour (along the 96 Avenue and 279 Street Road frontage);
- Enhanced landscaping requirements (along the 96 Avenue and 279 Street Road frontage);
- Water conservation measures;
- Screening / fencing requirements relative to existing adjacent business developments adjacent.

The developer will register the architectural guidelines against the certificate of title to each business lot to inform future owners of their development obligations.

<i>Policy 8.6.1</i>	The developer shall prepare architectural guidelines at the detailed design application approval stage to ensure the form of development maintains a coordinated and cohesive built form.
<i>Policy 8.6.2</i>	The developer will register the architectural guidelines against the certificate of title to each business lot to inform future owners of their development obligations.

9. SUMMARY OF POLICY STATEMENTS

<i>Policy 1.3.1</i>	Policies contained within the Conceptual Scheme shall apply to all lands within the Plan area.
<i>Policy 2.1.1</i>	The Conceptual Scheme and the land development it generates shall conform to both the Edmonton Metropolitan Region Growth Plan and the Municipal Government Act.
<i>Policy 2.2.1</i>	The Conceptual Scheme and the land development it generates shall conform to Parkland County's Municipal Development Plan.
<i>Policy 3.4.1</i>	Should any historical resources be encountered during construction, Parkland County and Alberta Culture and Tourism (Historic Resource Management Branch) shall be contacted immediately.
<i>Policy 4.2.1</i>	The Developer shall submit a detailed geotechnical analysis at the detailed design stage, to be prepared by a qualified Geotechnical Engineer, to confirm the suitability of subsurface conditions in accordance with the requirements of Parkland County's Engineering Design Standards.
<i>Policy 4.3.1</i>	Subdivision applications and development permits within the Plan area shall be consistent with the recommendations outlined in the Biophysical Assessment (WSP – February 2024), to the satisfaction of Parkland County.
<i>Policy 4.3.2</i>	Subdivision applications and development permits within the Plan area shall be consistent with the recommendations outlined in the Wetland Assessment and Impact Report (WSP – April 2024), to the satisfaction of Parkland County.
<i>Policy 5.3.2.1</i>	In accordance with Parkland County Policy C-PD15, dedication of Municipal Reserve (MR) shall be provided by the developer prior to subdivision endorsement via cash-in-lieu payment pursuant to the provisions of the Municipal Government Act.
<i>Policy 5.4.1</i>	Wetland disturbances shall proceed in accordance with the procedural, technical, and compensation requirements established by the Alberta Provincial Wetland Policy to the satisfaction of Alberta Environment & Protected Areas prior to subdivision endorsement.
<i>Policy 5.4.2</i>	The developer shall obtain the necessary Water Act approvals prior to construction and/or development permit where applicable to the subject lands and such approvals shall be provided to Parkland County.
<i>Policy 5.4.3</i>	Wetland compensation arrangements with the Province shall be established prior to subdivision application where applicable to the subject lands.
<i>Policy 5.4.4</i>	Any clearing of vegetation should occur between September 1 through to January 31 and clearing of vegetation beyond this timeframe shall require the employment of a qualified biologist to verify that no nesting migratory birds are harmed or disturbed.
<i>Policy 6.2.1</i>	Access will be provided to the Plan area as generally illustrated on Figure 8: Transportation.
<i>Policy 6.2.2</i>	The developer shall construct the portion of 279 Street between 92 Avenue and 96 Avenue as a two-lane industrial road, at the subdivision stage, to the satisfaction of Parkland County.
<i>Policy 6.2.3</i>	The future roadway network and key intersections in the Plan area shall be designed and constructed to accommodate all roadway users at a safe and satisfactory level of transportation services.
<i>Policy 6.2.4</i>	Subdivision applications and development permits within the Plan area shall be consistent with the recommendation as outlined in the Traffic Impact Assessment (McElhanney – October 31, 2024, as amended), to the satisfaction of Parkland County.

<i>Policy 6.2.5</i>	The Developer shall provide payment of Parkland County's Off-Site Transportation Levy at the subdivision and/or development permit stage.
<i>Policy 6.3.1</i>	The design of the internal subdivision road network, including intersections with the surrounding municipal road network, shall be established at the subdivision stage in accordance with the County's Engineering Design Standards. Changes to the internal road layout identified in Figure 8: Transportation shall not require amendments to this Conceptual Scheme, as long as the internal road is classified as a Local Road. Any changes to a Collector Road shall require an amendment to this Conceptual Scheme.
<i>Policy 6.3.2</i>	All dead-end roads, whether temporary or permanent, shall include a cul-de-sac turnaround, with a minimum radius as required under Parkland County's Engineering Design Standards.
<i>Policy 7.1.1</i>	The developer, prior to subdivision endorsement, shall be required to enter into a Development Agreement with Parkland County regarding all off-site infrastructure and construction.
<i>Policy 7.1.2</i>	Subdivision applications and development permits within the Plan area shall be consistent with the recommendations outlined in the Sanitary and Water Servicing Report (V3 Companies – October 18, 2024), to the satisfaction of Parkland County.
<i>Policy 7.2.1</i>	Potable water service shall be provided within the Plan area as generally illustrated by Figure 9: Water Servicing.
<i>Policy 7.2.2</i>	The water distribution network shall be designed and constructed by the developer at the subdivision stage in accordance with the Parkland County Engineering Design Standards and the most current Acheson and Big Lake Area Water Servicing Study.
<i>Policy 7.2.3</i>	The water distribution network shall be owned and maintained by the County.
<i>Policy 7.2.4</i>	The design of the water distribution network shall accommodate fire suppression in accordance with the Parkland County Engineering Design Standards.
<i>Policy 7.2.5</i>	At the time of subdivision, the Developer shall provide modelling data for water services. This data will be provided to the County's engineering consultant maintaining the servicing models for the Acheson area, and any required improvements identified in the updated model shall be required as a condition of subdivision approval.
<i>Policy 7.2.6</i>	The Developer shall provide payment of Parkland County's Off-Site Water Levy at the subdivision and/or development permit stage.
<i>Policy 7.3.1</i>	Wastewater service shall be provided within the Plan area as generally illustrated by Figure 10: Wastewater Servicing.
<i>Policy 7.3.2</i>	The wastewater collection network shall be designed and constructed by the developer at the subdivision stage in accordance with the Parkland County Engineering Design Standards and the most current Acheson and Big Lake Area Sanitary Sewer Servicing Study.
<i>Policy 7.3.3</i>	The wastewater collection network shall be owned and maintained by the County.
<i>Policy 7.3.4</i>	At the time of subdivision, the Developer shall provide modelling data for sanitary services. This data will be provided to the County's engineering consultant maintaining the servicing models for the Acheson area, and any required improvements identified in the updated model shall be required as a condition of subdivision approval.
<i>Policy 7.3.5</i>	The Developer shall provide payment of Parkland County's Off-Site Sanitary Sewer Levy at the subdivision and/or development permit stage.

<i>Policy 7.4.1</i>	Stormwater management shall be provided within the Plan area as generally illustrated on Figure 11: Stormwater Management. The final location and size of the stormwater pond shall be determined at detailed design.
<i>Policy 7.4.2</i>	The developer shall provide a Site-Specific Stormwater Implementation Plan (SSIP) at the detailed design stage to ensure positive drainage conditions are maintained within the Plan area.
<i>Policy 7.4.3</i>	The design of the stormwater management system within the Plan area shall be in accordance with AEP Approval 00287756-00-00 'Fenceline Boundary Agreement' and shall accommodate the unit area release rates and volume retention targets established by the Acheson and Big Lake Master Drainage Plan, 2011.
<i>Policy 7.4.4</i>	The stormwater management facility shall be designed and constructed by the developer at the subdivision stage in accordance with the Parkland County Engineering Design Standards.
<i>Policy 7.4.5</i>	The stormwater management system shall be designed and constructed to meet or exceed Parkland County's Engineering Design Standards as well as Alberta Environment and Protected Area stormwater management and stormwater quality requirements for the Acheson Big Lake Storm Basin.
<i>Policy 7.4.6</i>	The stormwater management facility shall be dedicated to Parkland County within a Public Utility Lot (PUL) at the subdivision stage.
<i>Policy 7.4.7</i>	All stormwater management infrastructure within the Plan area will be owned and operated by the County.
<i>Policy 7.4.8</i>	At the time of subdivision, the Developer shall provide modelling data for stormwater services. This data will be provided to the County's engineering consultant maintaining the servicing models for the Acheson area, and any required improvements identified in the updated model shall be required as a condition of subdivision approval.
<i>Policy 7.4.9</i>	The Developer shall provide payment of Parkland County's Off-Site Stormwater Levy at the subdivision and/or development permit stage.
<i>Policy 7.5.4.1</i>	The developer shall prepare a Waste Management Plan at the detailed design stage, to the satisfaction of the County.
<i>Policy 7.5.4.2</i>	The developer shall establish A Business Lot Owner Association at the detailed design stage to manage a contract with a qualified waste management provider.
<i>Policy 7.6.1</i>	Shallow franchise utilities shall be installed and/or financed by the developer at the subdivision stage in consultation with the applicable utility providers.
<i>Policy 7.6.2</i>	The alignments for franchise utility installations shall be determined at the subdivision stage in accordance with Parkland County Engineering Design Standards and the requirements of the applicable franchise utility operator.
<i>Policy 7.7.1</i>	Prior to stripping and grading within the Plan area, a grading plan and erosion and sediment control report shall be submitted to the satisfaction of Parkland County. This plan shall depict road and swale grades and include public utility lot areas, stormwater management plans, and an erosion and sediment control plan and shall be approved by Parkland County.
<i>Policy 7.7.2</i>	A stripping a grading permit issued by Parkland County shall be in place prior to any soils being removed from the Plan area.

<i>Policy 8.1.1</i>	Prior to any land development activities occurring within the Plan area, amendments to the designation of the lands within Map 6: Future Land Use Concept of the Acheson Industrial Area Structure Plan shall be completed to ensure the future land use designation is consistent with the designated purposes outlined in Figure 7: Development Concept of this Conceptual Scheme.
<i>Policy 8.2.1</i>	Prior to any land development activities occurring within the Plan area, amendments to the districting of Parkland County's Land Use Bylaw shall be completed to ensure the districting is consistent with the designated purposes outlined in Figure 13: Proposed Land Use of this Conceptual Scheme.
<i>Policy 8.3.1</i>	Subdivision within the Plan area is expected to proceed as generally illustrated on Figure 14: Proposed Subdivision.
<i>Policy 8.3.2</i>	Future lot sizes and configurations shall be determined at the time of subdivision. Changes to the proposed lot layout identified in Figure 14: Proposed Subdivision shall not require amendments to this Conceptual Scheme.
<i>Policy 8.4.1</i>	The development within the Plan area is expected to proceed in one (1) phase as generally illustrated on Figure 15: Phasing.
<i>Policy 8.4.2</i>	An alternate phasing program may be permitted without an amendment to this Plan provided there is appropriate infrastructure available to support each phase, including any temporary second access/egress point requirements if needed.
<i>Policy 8.5.1</i>	A Business Lot Owner's Association shall be established at the detailed design stage for the purposes of managing a contract with a waste management provider and other duties as required.
<i>Policy 8.6.1</i>	The developer shall prepare architectural guidelines at the detailed design application approval stage to ensure the form of development maintains a coordinated and cohesive built form.
<i>Policy 8.6.2</i>	The developer will register the architectural guidelines against the certificate of title to each business lot to inform future owners of their development obligations.

10. LIST OF FIGURES

- Figure 1 - Regional Context
- Figure 2 – Local Context
- Figure 3 – Legal Descriptions
- Figure 4 – Existing Land Use
- Figure 5 – Existing Conditions
- Figure 6 – Topography
- Figure 7 – Development Concept
- Figure 8 – Transportation
- Figure 9 – Water Servicing
- Figure 10 – Wastewater Servicing
- Figure 11 – Stormwater Management
- Figure 12 – Open Space
- Figure 13 – Proposed Land Use
- Figure 14 – Proposed Subdivision
- Figure 15 – Phasing

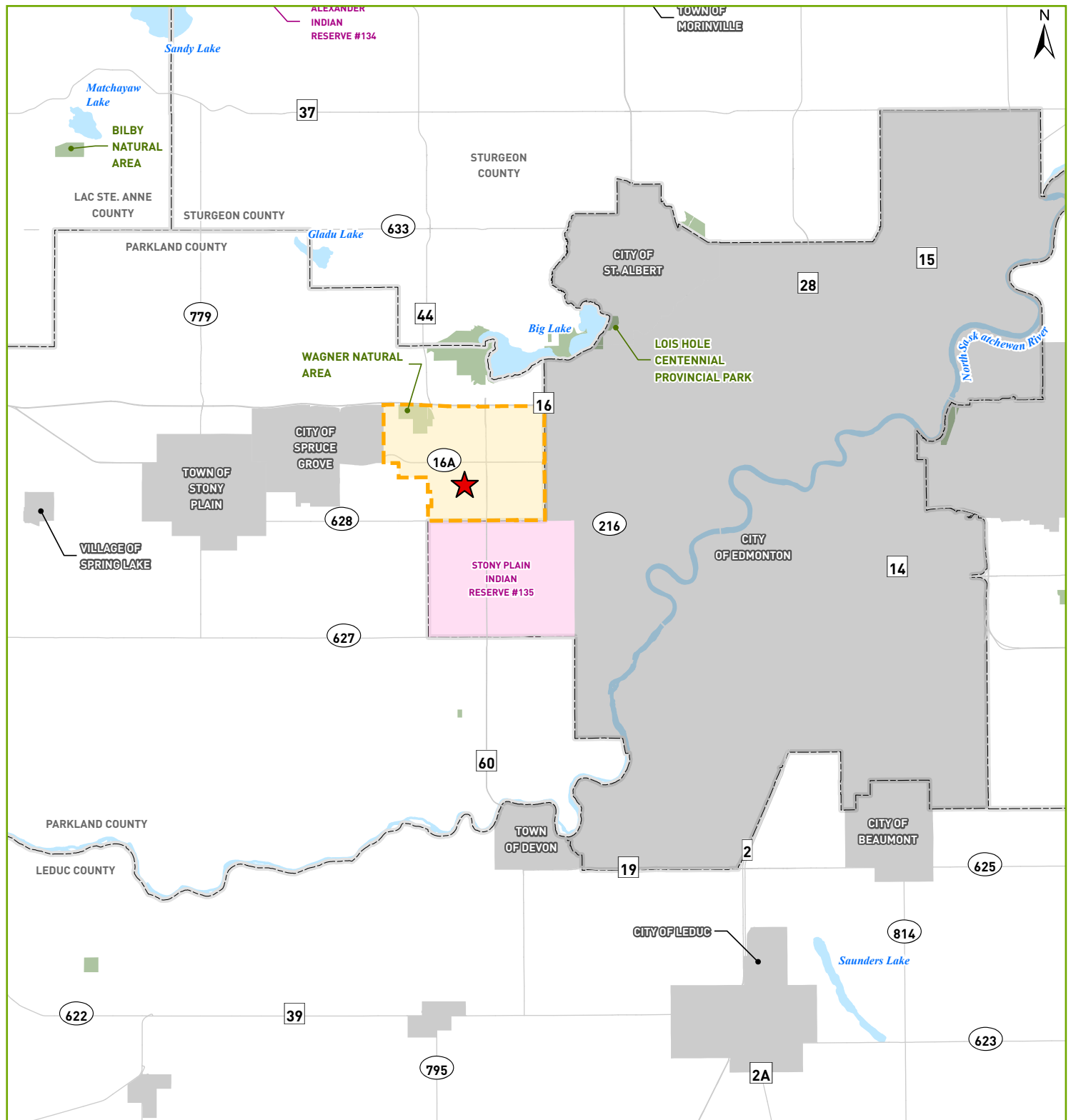


FIGURE 01: Regional Context



Conceptual Scheme Area

Major Road

Acheson Industrial ASP Boundary

First Nation/Métis Settlement

Municipality - Rural

Municipality - Urban

Park/Protected Area

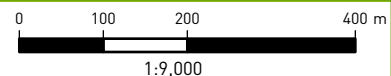
Waterbody








parkland
county



FIGURE 02: Local Context



- | | | |
|---|---|--|
|  Conceptual Scheme Area |  Legal |  Quarter Section Grid |
|  Neighbouring Company |  Road | |



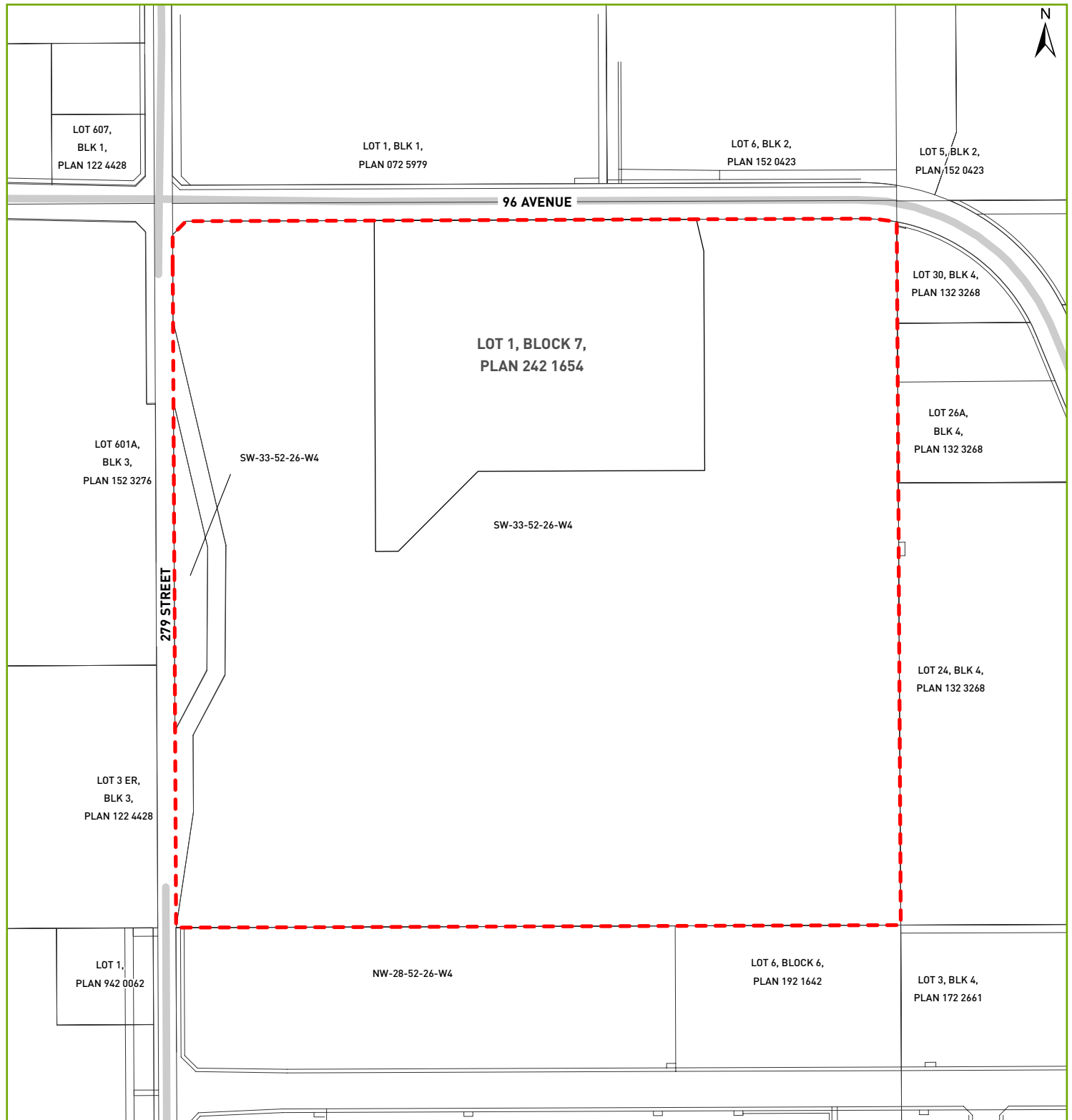





FIGURE 03: Legal Description

 Conceptual Scheme Area
 Legal
 Road



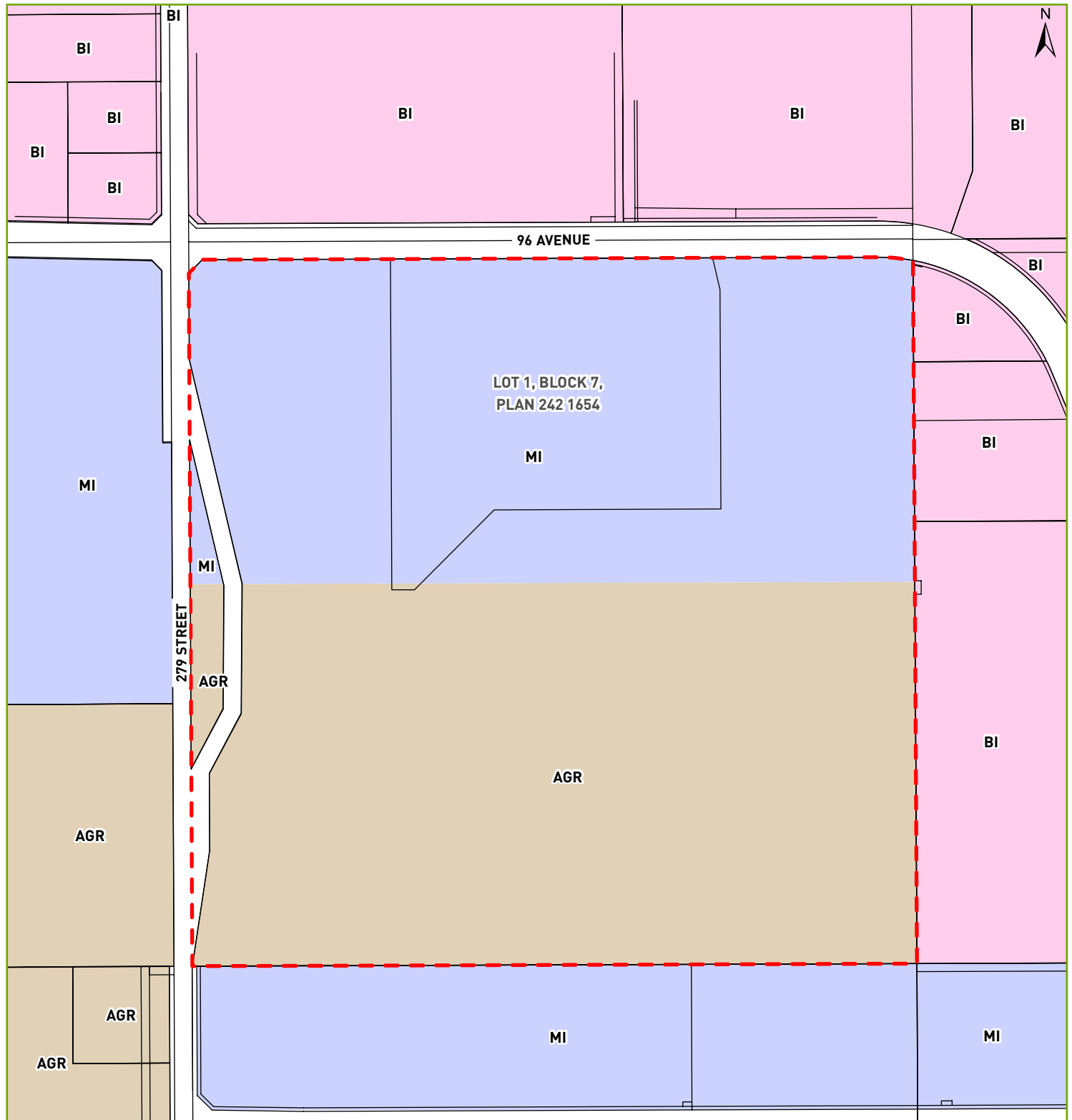
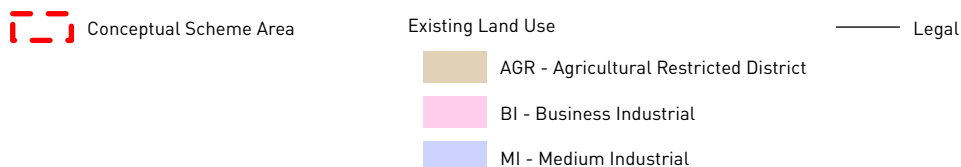
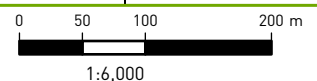


FIGURE 04: Existing Land Use



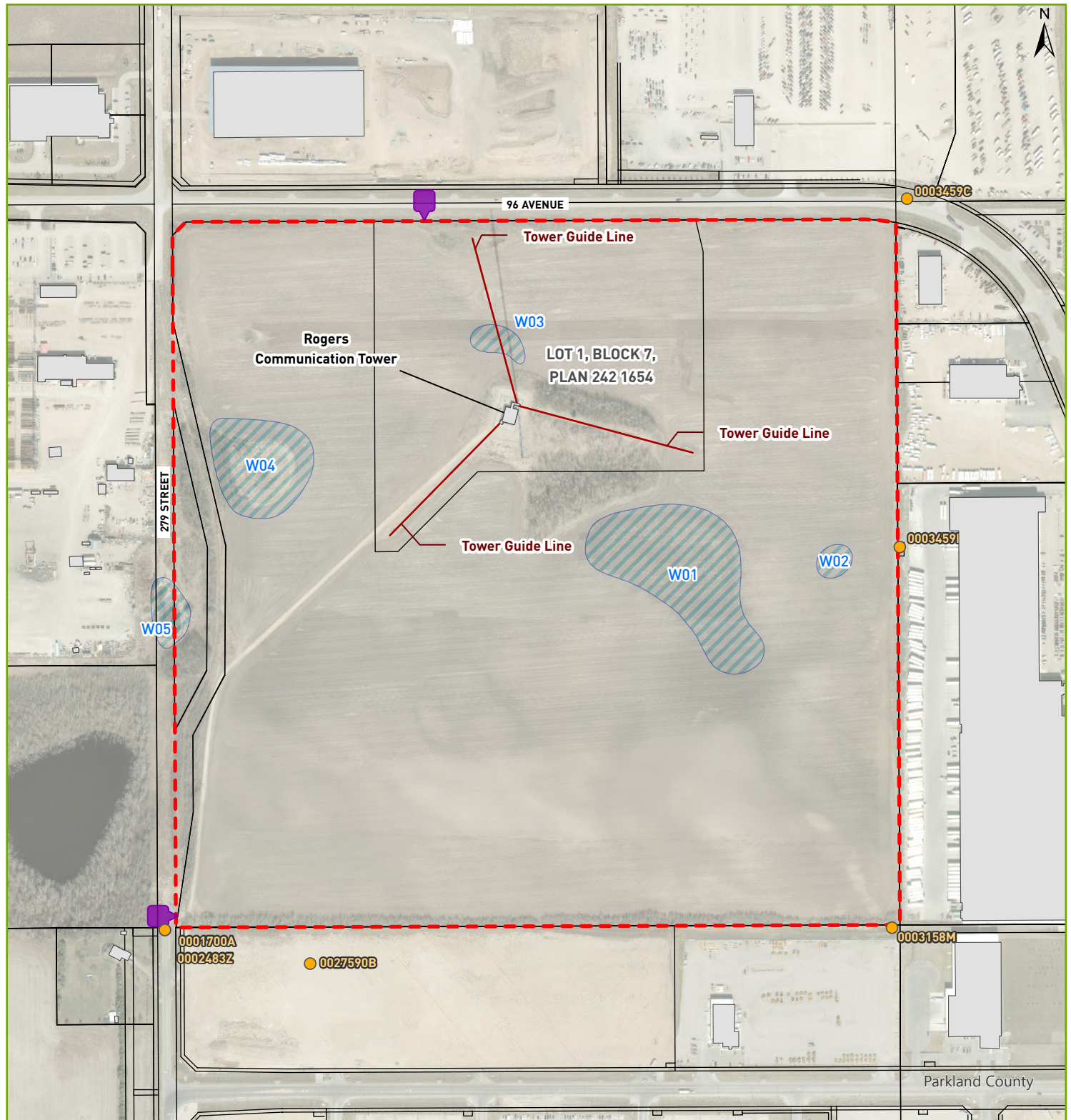
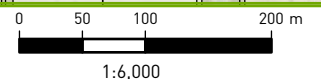


FIGURE 05: Existing Conditions



- Conceptual Scheme Area
- Legal
- Wetland
- Abandoned Oil & Gas Well
- Tower Guide Line
- Existing Approach
- Existing Structure



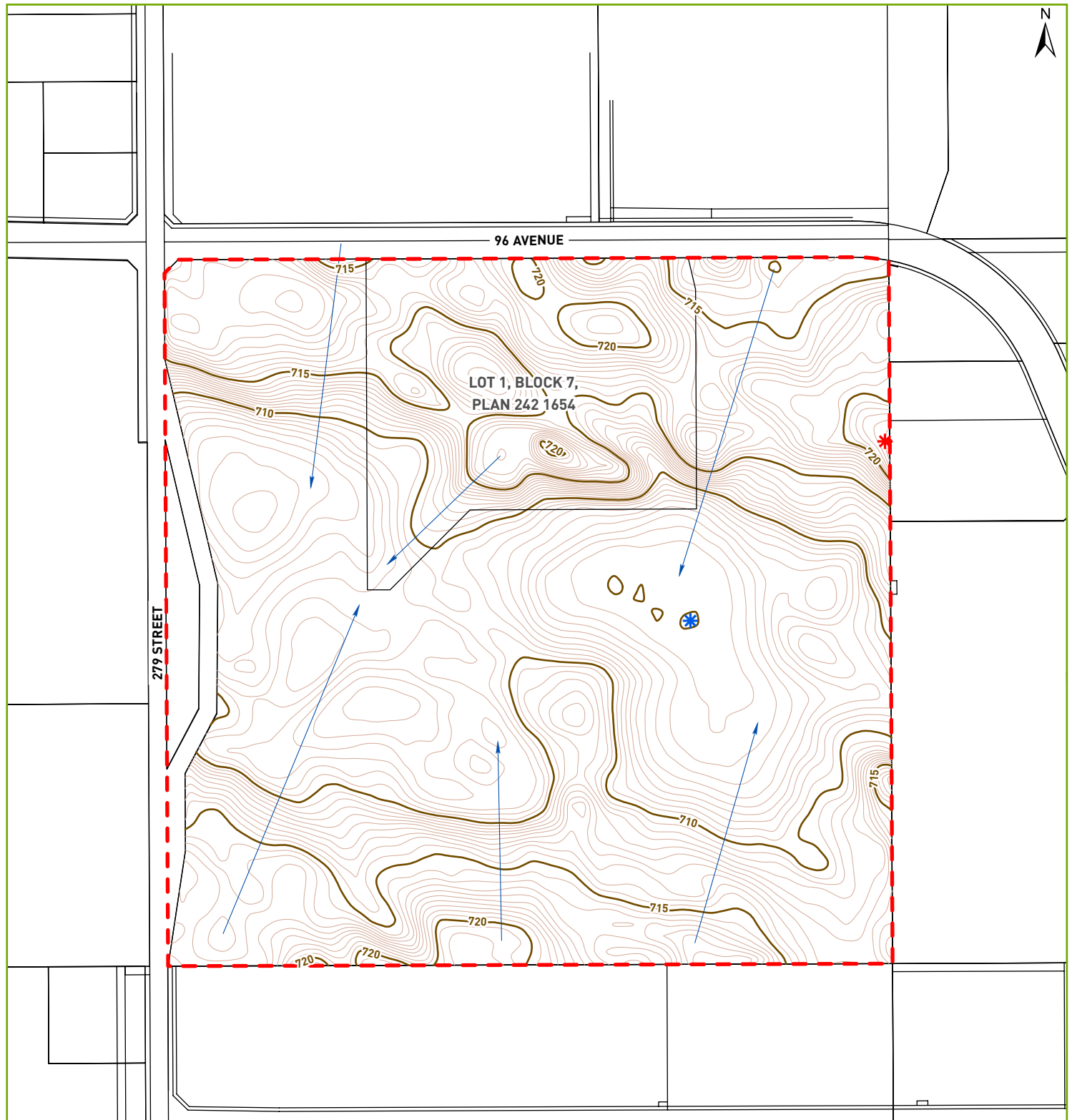
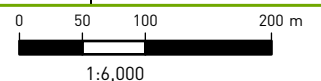


FIGURE 06: Topography

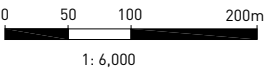


- | | | |
|------------------------|-----------------------------|--------------------|
| Conceptual Scheme Area | Low Point (705m) | Index Contour (5m) |
| High Point (722.5m) | Intermediate Contour (0.5m) | Legal |
| Flow Direction | | |





FIGURE 07: Development Concept



--- Conceptual Scheme Area



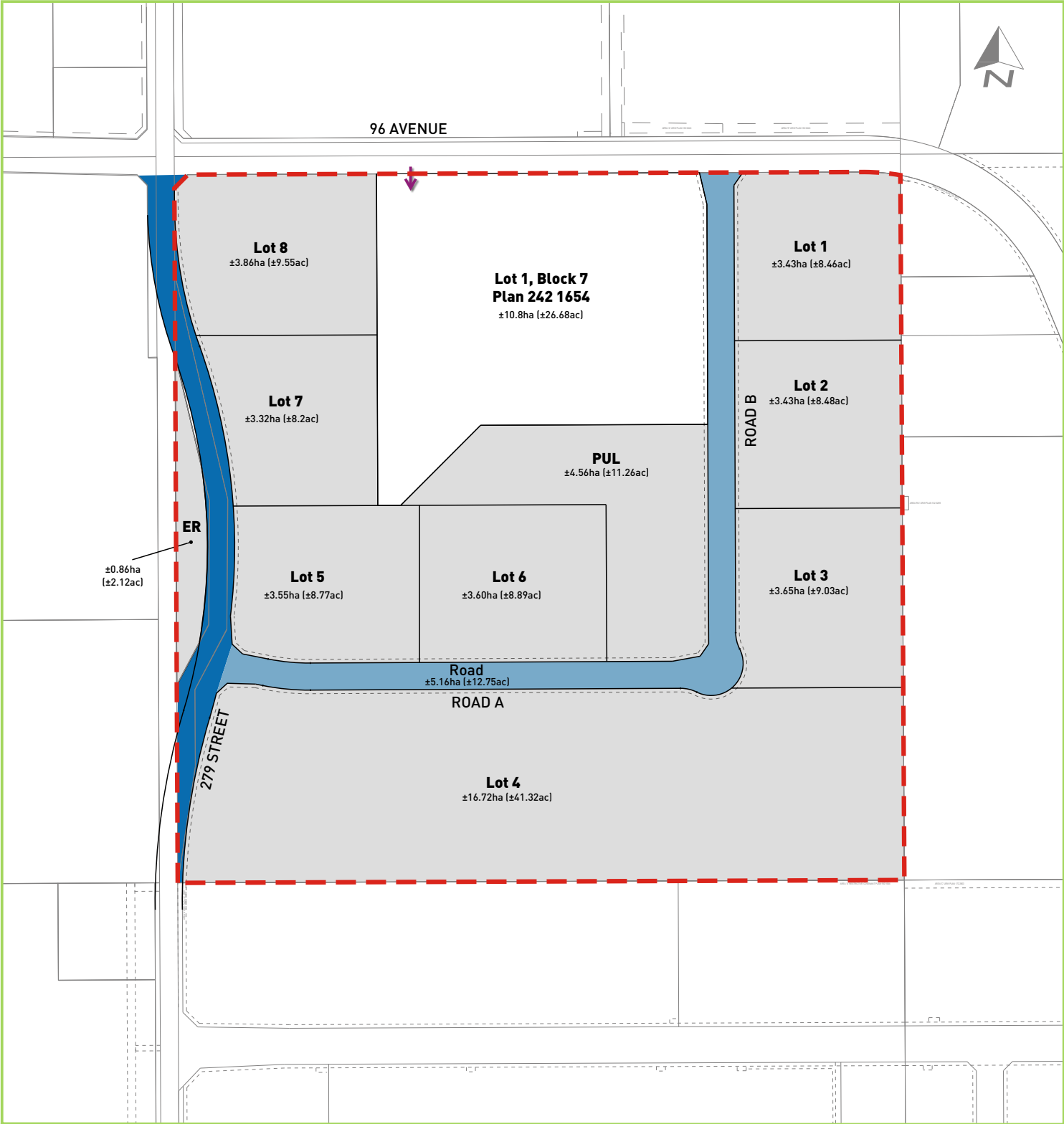
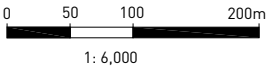


FIGURE 08: Transportation



- Conceptual Scheme Area
- ➡ Approach Site Access
- Lot 1, Block 7, Plan 242 1654
- 12.75m Local Road
- 30m Collector Road
- Proposed Subdivision Area



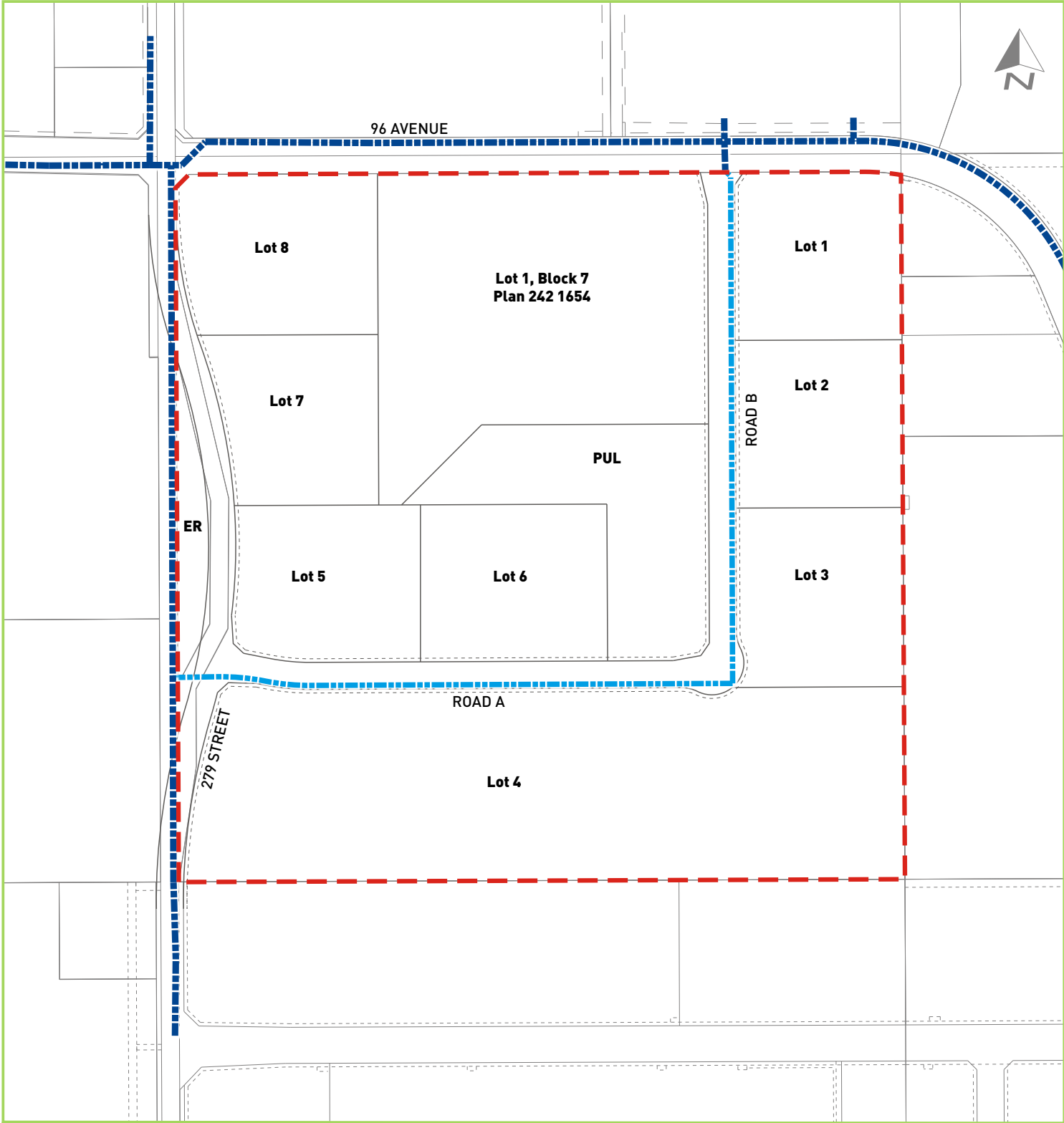


FIGURE 09: Water Servicing

- Conceptual Scheme Area
- Water Servicing Proposed 300mm Water Main
- Water Servicing Existing 400mm Water Main



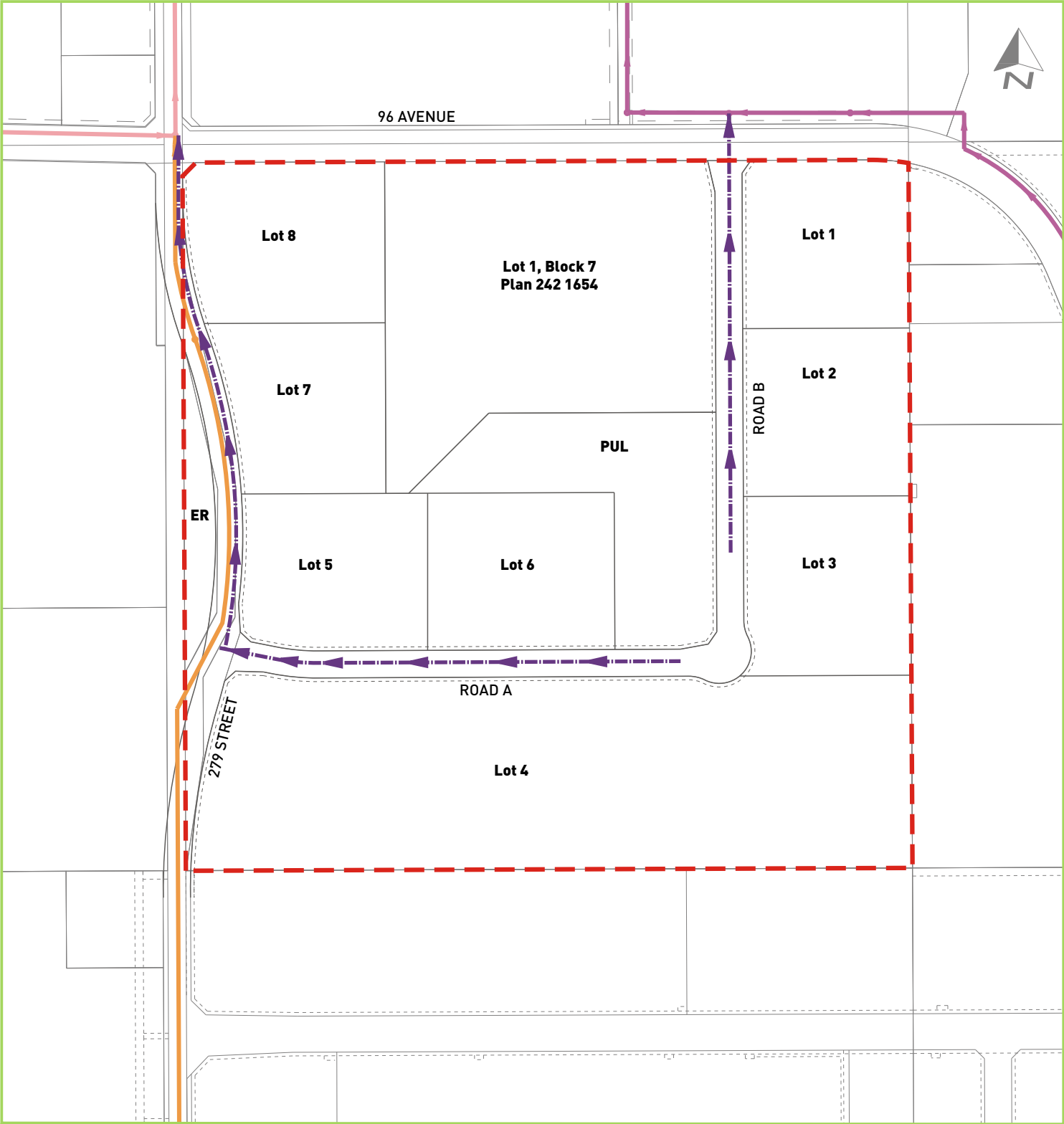
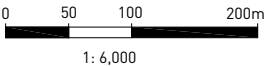


FIGURE 10: Wastewater Servicing



- Conceptual Scheme Area
- Existing 250mm Sanitary Servicing
- Proposed 250mm Sanitary Servicing
- Existing 350mm Sanitary Servicing
- Existing 450mm Sanitary Servicing



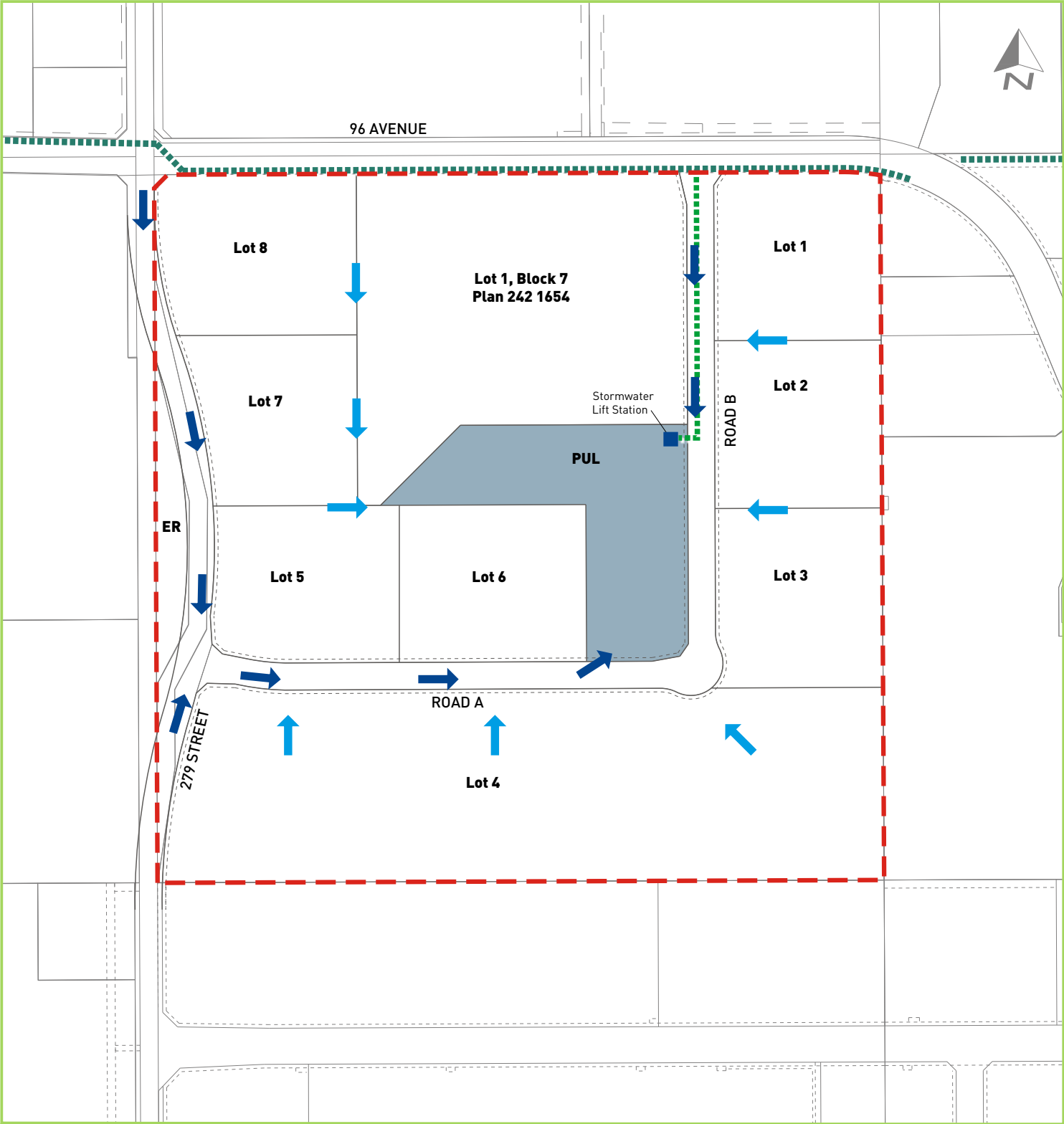
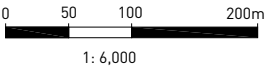


FIGURE 11: Stormwater Management



- Conceptual Scheme Area
 - Lot Drainage
 - Road Ditch Drainage
 - Public Utility Lot (PUL)
 - Proposed Storm Servicing Lift Station and Internal Force Main
 - Existing Storm Servicing Internal Force Main
 - Stormwater Lift Station
- Note: SWMF will be Located within the PUL





FIGURE 12: Open Space

- Conceptual Scheme Area
- Environmental Reserve
- Public Utility Lot / Stormwater Management Facility



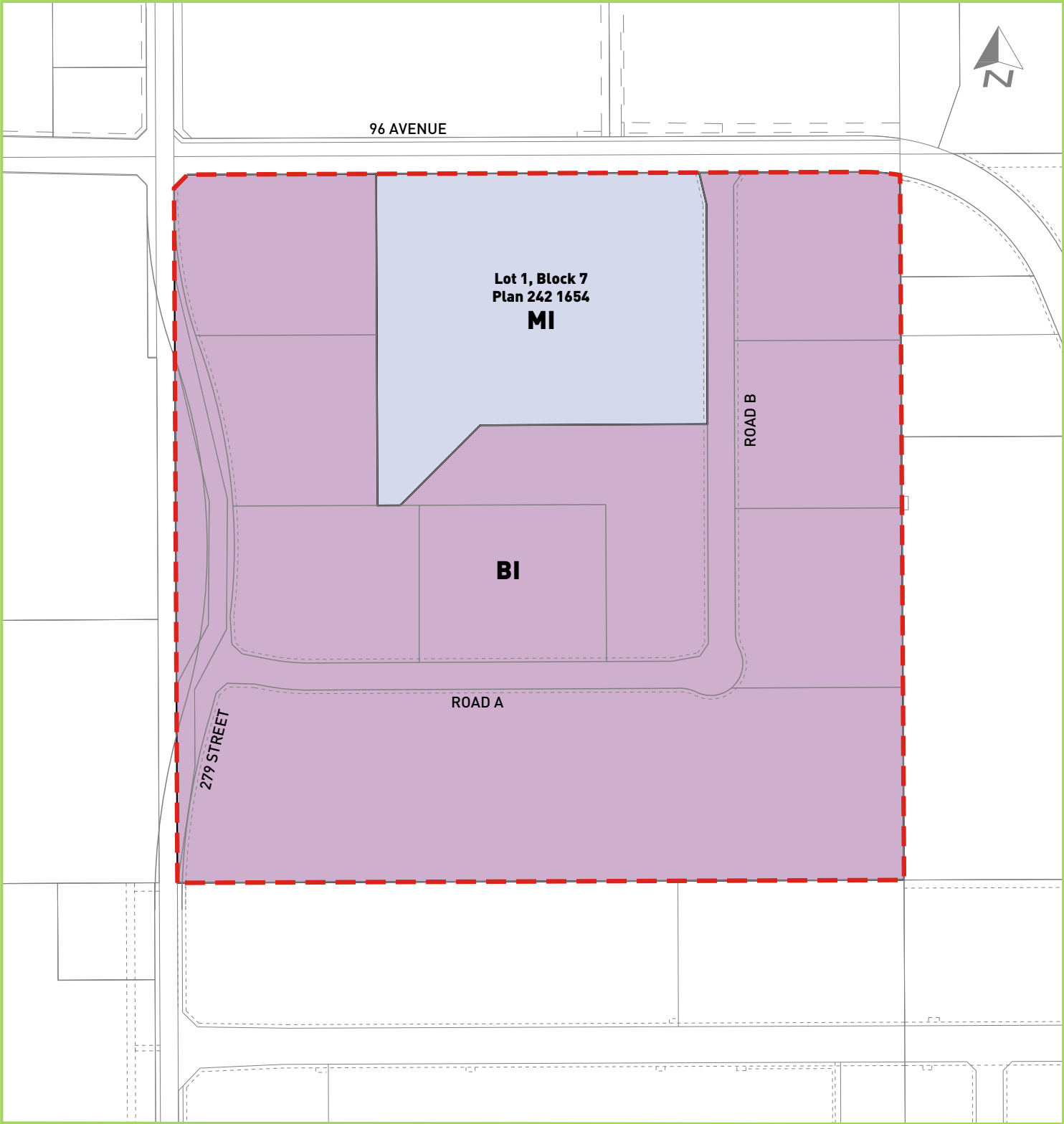
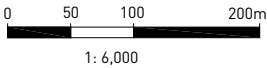


FIGURE 13: Proposed Land Use



- Conceptual Scheme Area
- BI - Business Industrial District (Proposed)
- MI - Medium Industrial District (Existing)



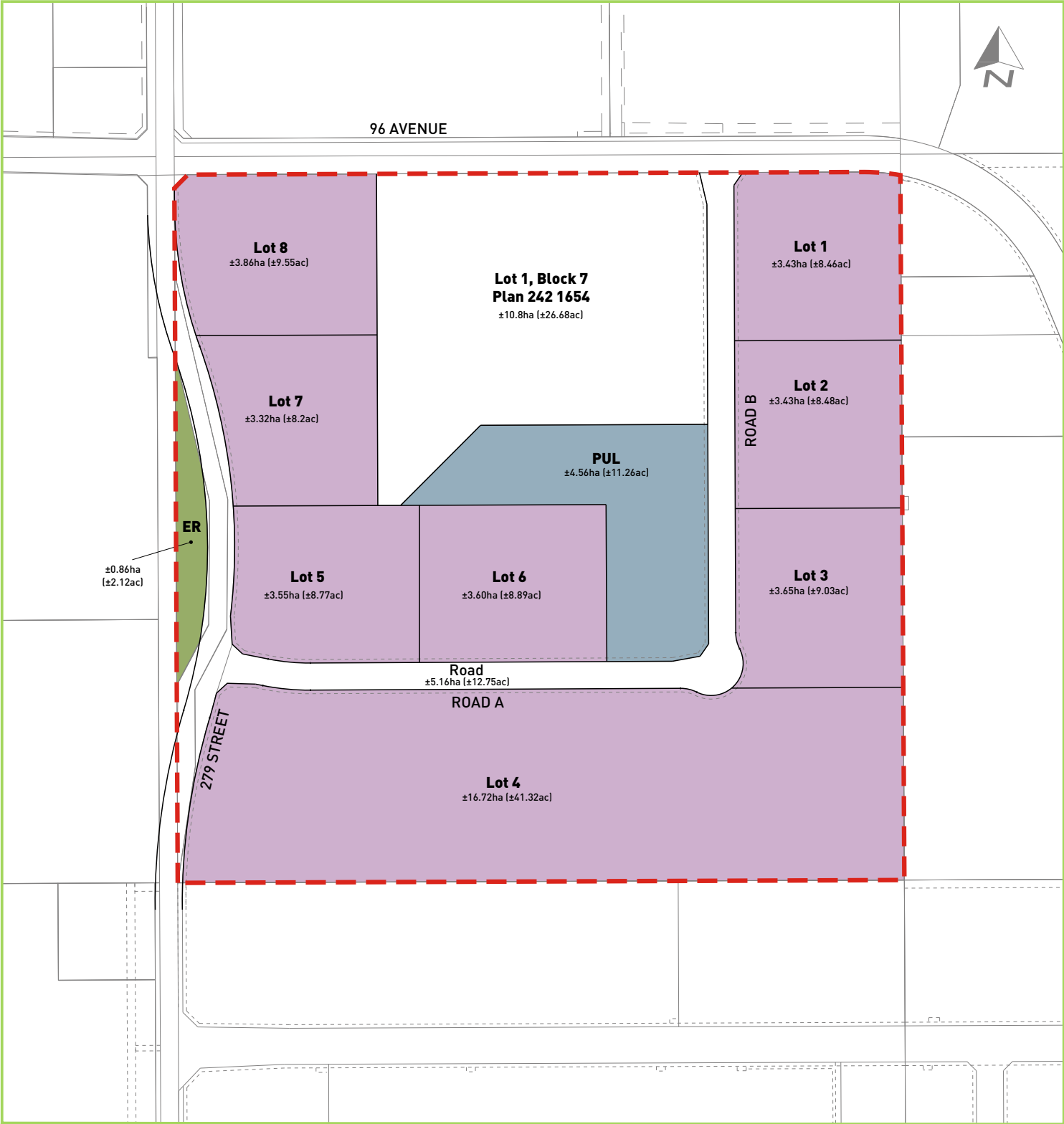
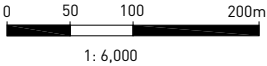


FIGURE 14: Proposed Subdivision



- Subject Lands
- Industrial
- Public Utility Lot
- Environmental Reserve



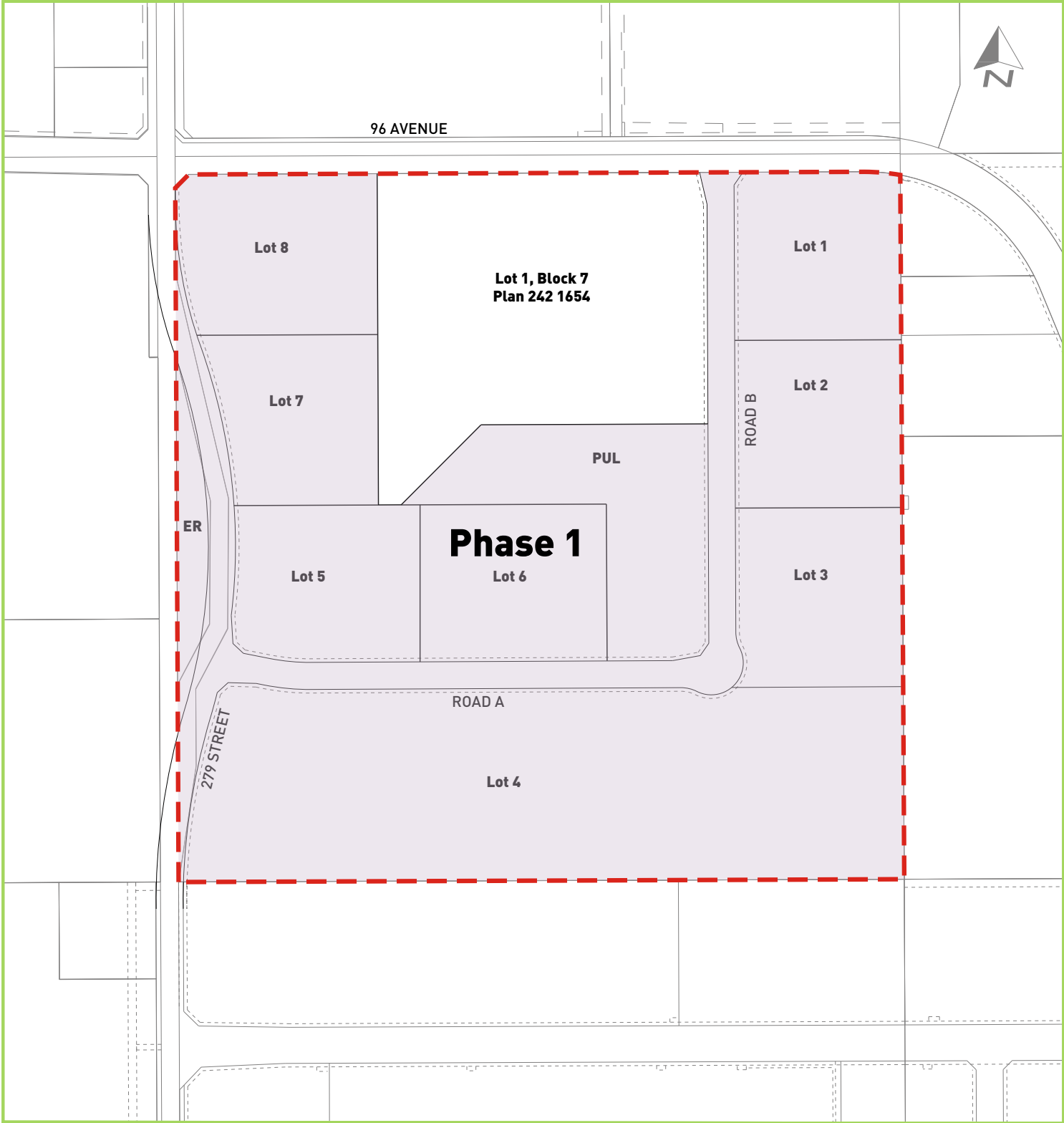
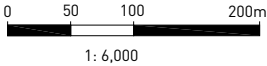


FIGURE 15: Phasing



--- Conceptual Scheme Area Phase 1 Area

